



Cooktown Foreshore & Webber Esplanade Revitalisation

Master Plan Report

Prepared by **Tract Consultants Pty Ltd**
In conjunction with **Aecom/RLB**

Client: **Cook Shire Council**

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Project Number: 0711-0310

Executive Summary

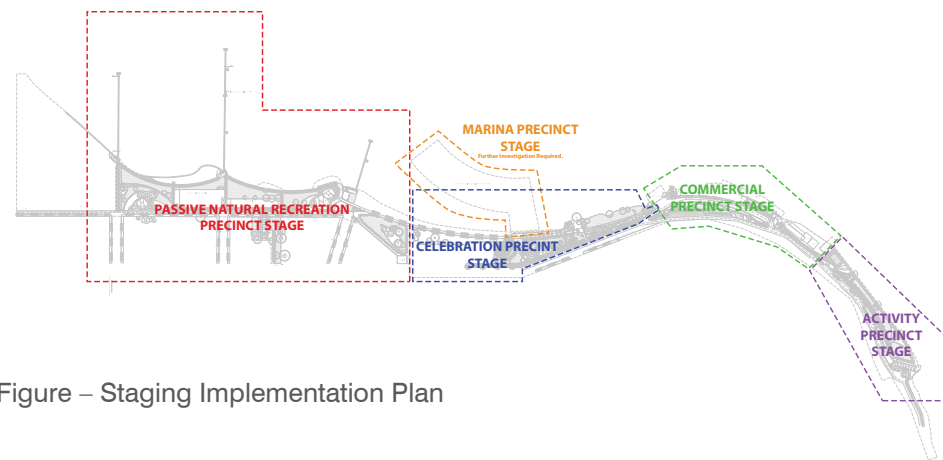


Figure – Staging Implementation Plan

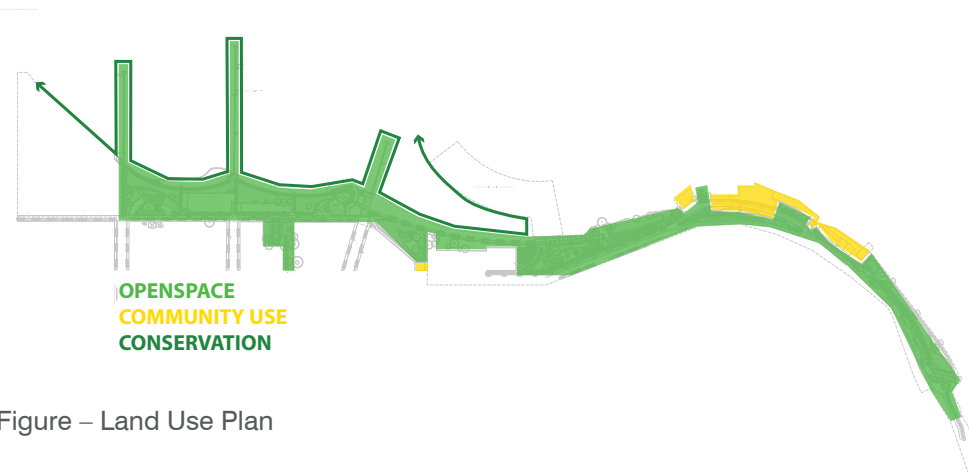


Figure – Land Use Plan

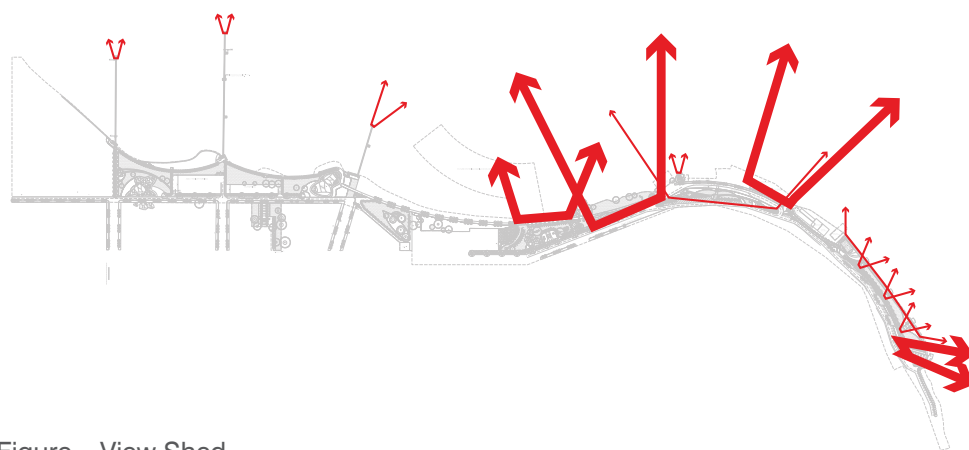


Figure – View Shed

Cook Shire Council (CSC) have commissioned Tract Consultants to develop a Foreshore Revitalisation Master Plan and Report for the Cooktown Foreshore. This plan will provide a strategic vision for the future development and revitalisation of Cooktown's foreshore that proposes to reconnect the residents and locals with the waterfront. One of the main drivers of the Master Plan is to revitalise the town through private investment, on the back of public investment in the foreshore.

The proposed Master Plan includes:

- Development of precincts to create legibility and define function, activity and opportunity;
- Strong linkages to upgrade and create clear, direct and equitable shared pathways; and
- Equidistant placement of points of interest that will sustain the length of the user's experience and strengthen the identity of Cooktown.

In order for CSC to achieve their 'shovel ready' approach to sourcing funding and progress with the development of their foreshore, the proposed landscape and infrastructure upgrades, along with their associated costs, have been illustrated in this report.



Cooktown Foreshore & Webber Esplanade Revitalisation



Figure – Scale 1:4000 - Cooktown Foreshore & Webber Esplanade Master Plan



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Disclaimer

This document has been prepared for the purpose of enlightened discussion and does not imply commitment to its implementation by any party. No part of this document shall be reproduced in any format without the express written permission of Cook Shire Council or Tract Consultants. This document has been based on reasonable assumptions and is yet to be subject to detailed planning, design and engineering assessment. This document is subject to revision.



1 Background Context

1.1 Cooktown's Presence within Australia

Cooktown plays a vital role in the history of the exploration of Australia. British explorer Captain James Cook discovered Australian shores in 1770, experiencing his first interaction with indigenous people and kangaroos. Cooktown is an iconic destination positioned at the top of Queensland. It is very rich in history and culture that has contributed to and shaped the evolution of Australia.

1.2 Cooktown's Presence within Queensland

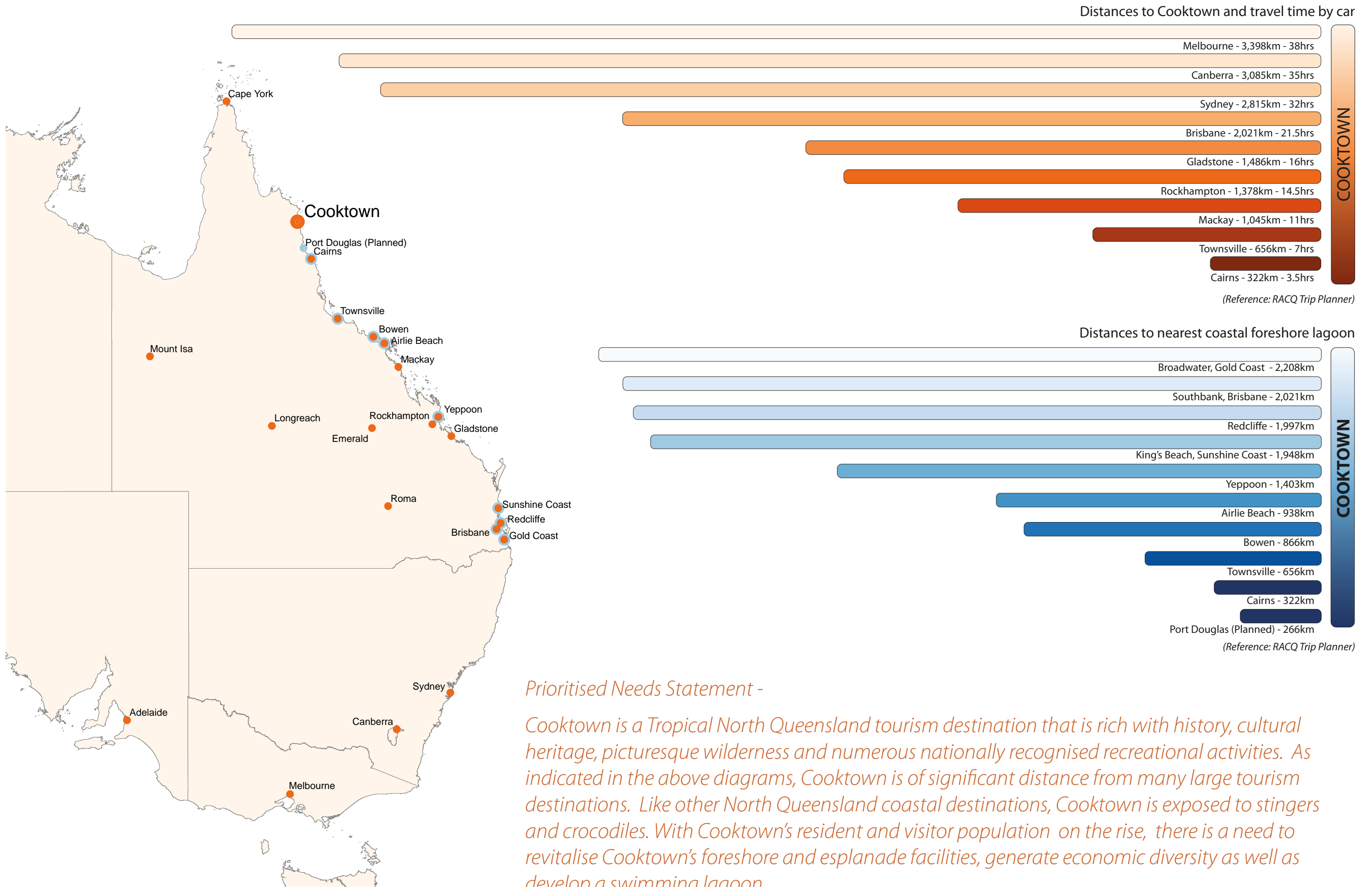
Cooktown is the gateway to the Cape York landscape and tourism, the northern most point of Australia's Wet Tropics and World Heritage Area, and is accessible by land, air and sea. The town is approximately 322 kilometres by road from Cairns and 2,021 kilometres north of Brisbane. The Great Barrier Reef is approximately 20 minutes by boat from the Endeavour River. Cooktown is known for its exceptional riparian and open water fishing, deep water on-shore fishing and reef snorkelling, attracting many seasonal Marlin boats and keeping the locals' dinner plates full year round.

1.3 Cooktown's Presence within the Region

Nestled between the Endeavour River and Coral Sea, Cooktown has amazing picturesque views to the water and beyond landscapes. Nature's Powerhouse, positioned amongst 60 hectares of reserve, is the region's official information centre. Cooktown is one of the largest settlements within the vast and broad region, attracting locals from surrounding villages to visit for recreational and business reasons.

A few facts about Cooktown are:

- Population of 2,093 persons approximately;
- 52.7% of population are male;
- 46.3% of the population are female;
- 11.8% of the population are indigenous;
- Median age of Cooktown residents is 42 years old;
- 27.8% of the population (584 people) is between the ages 0-24 years;
- 509 people have full-time employment;
- 261 people have part-time employment; and
- 527 people are unemployed.



1.4 Key Identified Stakeholders

This document describes the precincts along the foreshore and identifies a range of possible uses and activities appropriate to the character of that precinct. The uses were derived through consultation with a number of stakeholders as mentioned below. This Master Plan poses the opportunity to stimulate the Cooktown economy in a diverse manner in the future by identifying exactly what development and upgrades should go where. There are twelve groups of identified stakeholders that are currently associated with the Cooktown Foreshore and Webber Esplanade development. These stakeholders include:

- State and Commonwealth Agencies;
- Cook Shire Council;
- Community Groups;
- Traditional Owners;
- Residents;
- Leaseholders and Landholders;
- Visitors;
- Tourism Stakeholders;
- Tourism Industry;
- Ports Corporation;
- Industry Representatives. ie. Fishing Industry; and
- Developer and Investors.

1.5 Waterfront Leases

The main waterfront leases that have influenced and shaped the evolution of this Master Plan are:

- The Australian Volunteer Coast Guard Association Incorporated;
- Fisherman’s Wharf;
- Lease A - E; and
- Cooktown Blue Water Club Incorporated.

1.6 Other Neighbouring Commercial Establishments

The following establishments identified are positioned on the western side of Charlotte Street. This list is not inclusive yet does include the following businesses:

- Cooktown Seaview Hotel;
- Bowls Club;
- RSL;
- Cooktown School of Art Society;
- Westpac Banking Corporation and Post Office;
- Cooktown Orchid Travellers’ Park;
- Reef Cafe;
- The Cooktown Cafe;
- Discount Variety Store;
- Far North Photos; and
- Croc Shop - Cape York and Cooktown Souvenirs.



Figure – The Croc Shop - Cape York and Cooktown Souvenirs

2 Community Consultation

2.1 Approach

CSC have commissioned Tract Consultants to develop a Foreshore Revitalisation Master Plan and Report that proposes to reconnect the residents and locals with the waterfront to revitalise the town. One of the main drivers of the Master Plan is to revitalise the town through private investment, on the back of public investment in the foreshore. This will provide an economically diverse approach to sustaining the growth of the town. The proposed Master Plan includes:

- Development of precincts to create legibility and define function, activity and opportunity;
- Strong linkages to upgrade and create clear, direct and equitable shared pathways; and
- Equidistant placement of points of interests that will sustain the length of the user's experience and strengthen the identity of Cooktown.

Prior to the development of the Draft Concept Plan, CSC offered the community and stakeholders an opportunity to provide their input and ideas of what they would like to see along their foreshore. This involved a 'Community Consultation Information Period' that enabled the community to have their say through a feedback survey that was emailed, posted and available online.

For the full analysis, results and survey forms, please refer to the Cooktown Foreshore & Webber Esplanade Community Consultation Analysis Report issued January 2012.

2.2 Feedback & Analysis

2.2.1 Community Consultation Information Period

In order to establish what Cooktown and surrounding regional residents wanted or envisaged in their foreshore, CSC provided an opportunity for their vision, input, ideas and concerns to be voiced through a period of consultation, known as 'Community Consultation Information Period', running from 4 November to 18 November 2011. Feedback Surveys were emailed, posted and available online. This initial feedback identified that the best things about Cooktown's foreshore and esplanade were the views, fishing and relaxed character. The least liked things identified were parking around the wharf area and lack of shelter from the sun and wind. The survey also asked residents and stakeholders to prioritise the previously explored community project suggestions. The projects are listed in order of priority, with number 1 being the project they would most like to see included in the Master Plan.

Information Period - Overall Feedback Data	
Total Number Surveys of Emailed / Posted	Approximately 500
Total Number of Responses	52
Number of Residents that Responded	52
Number of Stakeholders that Responded	0
Information Period - Feedback Summary	
	Rank
Cooktown Wharf and Jetties Upgrade	1
Saltwater Tidal Lagoon	2
Marina Development	3
Lions Park Revitalisation	4
Captain Cook's Landing Area Revitalisation	5
Connecting Pathways and Access	6
Rail Shunting Yard Openspace	7
Reconciliation Rocks Revitalisation	8

2.2.2 Community Consultation Review Period

The main ideas and initiatives outlined in these surveys were then transformed into the Draft Concept Plan, that was later endorsed by Council at the November 2011 monthly meeting. From 26 November 2011, the Concept Design Package was available for community and stakeholders to comment on until 30 December 2011, in the form of:

- Feedback Survey;
- Online Feedback Form;
- Written Submissions (posted, emailed or Facebook);
- Verbal Survey and Feedback; and
- Displays: Markets and Library.

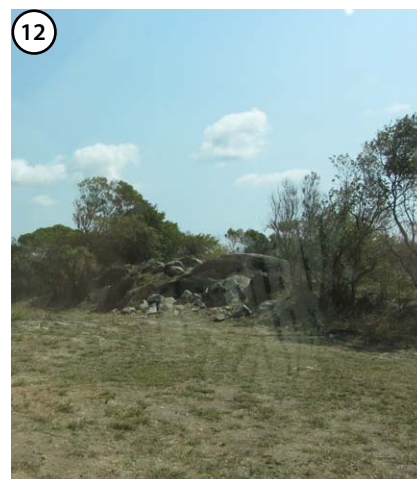
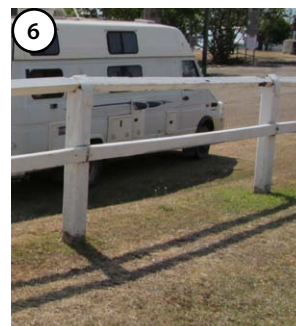
Review Period - Overall Feedback Data	
Market Stall Responses	10
Feedback Survey Responses	73
Submission Responses	11
Overview	
The best things about the foreshore and esplanade?	Views (29.0%)
The least liked about the foreshore and esplanade?	Limited Shade (7.0%)
Most liked to change on the foreshore and esplanade?	Lagoon / Water Play / Swimming Area
Project of highest priority from weighting analysis	Cooktown Wharf and Jetties Upgrade
Project most frequently voted as the number one priority	Saltwater Tidal Lagoon (35.5%)

Review Period - Overall Feedback Analysis	
Most liked to change on the foreshore and esplanade?	1. Increased Seating and Picnic facilities
	2. Car and trailer parking
	3. Historical Value - Preserve and Amplify
	4. Playgrounds and Exercise Equipment
	5. Shade - Structural and Natural
	6. Openspace Upgrade
	7. Access and Connectivity - Walkways / Cycleways
Summary of projects of highest priority (from weighting analysis) and most frequently voted as number one priority	Saltwater Tidal Pool and Water Play
	Cooktown Wharf and Jetties Upgrade -NOT IN MASTER PLAN SCOPE
	Connecting Pathways and Access
	Marina Development - NOT IN MASTER PLAN SCOPE
	Captain Cook's Landing Area Revitalisation
	Lion's Park Revitalisation
	Reconciliation Rocks Revitalisation
	Rail Shunting Yard Openspace

3 Existing Site Conditions

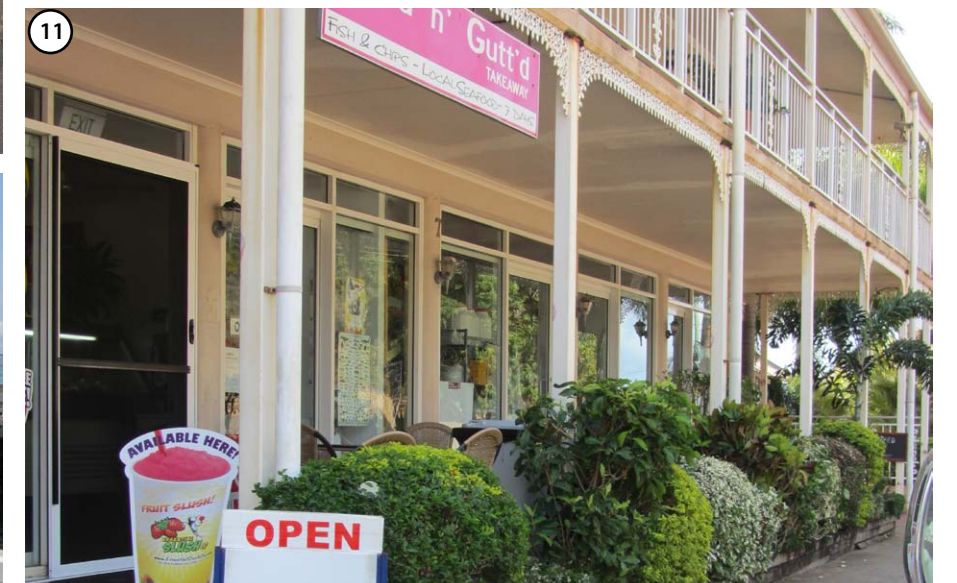
3.1 Some Items of Significance

1. Musical Ship, Endeavour Park
2. Reclamation Wall (under construction)
3. Town Well
4. Well, sourced by Grassy Hill Spring
5. Heritage granite kerb along Adelaide Street
6. White post and rail fence
7. Rail Shunting Yard remnants
8. Anzac Park
9. Milbi Wall Monument
10. Mick the Miner Monument
11. River of Life - community artistic pavers
12. Reconciliation Rocks
13. Cannon located beside Captain Cook Memorial, Endeavour Park
14. Statue of Captain Cook
15. Tree of Knowledge
16. Pilot's Jetty
17. Captain Cook Memorial Stone
18. Bicentennial Horse Trail Destination Point
19. Chinese Statues



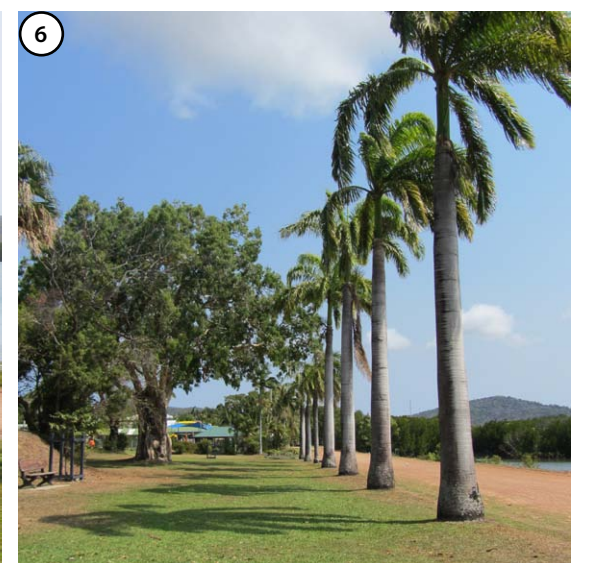
3.2 Some Existing Infrastructure / Services

1. Existing services – transformer
2. Current boat ramp facilities
3. Cooktown Bluewater Club Leaseholder -with white post and rail fence
4. Above ground power lines
5. Above ground power lines alongside wide road reserve
6. Existing oil trap at wharf area
7. Existing toilet and waste disposal facility at wharf area
8. Current condition of storm water drainage at southern end of site
9. Existing jetty facilities
10. Existing wharf facilities
11. Current waterfront lease holder, Fisherman's Wharf
12. Current drainage and storm water outlet as well as skate drain
13. Current Lions Park sheltered gas barbeque and gazebo (background)
14. Other Commercial Establishments
15. The Australian Volunteer Coast Guard Incorporated Leaseholder
16. Playground, Lion's Park



3.3 Viewsheds

1. View heading north along Adelaide Street
2. View from Adelaide Street through to Charlotte Street
3. Bicentennial Park looking over Endeavour River to North Shore
4. Vast and open view from jetties and wharves
5. View from Lions Park towards mangroves up Endeavour River
6. View heading south down Adelaide Street through Lions Park
7. Looking north up Adelaide Street to the mouth of the river
8. Pilot's Jetty towards mouth of river
9. Powder Magazine towards water's edge



Cooktown Foreshore & Webber Esplanade Revitalisation

3.4 Wayfinding, Directional, Interpretive Signage

1. Chinese Monument information signage
2. Musical Ship precautionary information signage
3. Cooktown Foreshore interpretive signage suite
4. Generic information signage
5. Cooktown Scenic Rim Self Guided signage Suite
6. Cooktown Scenic Rim Self Guided signage Suite
7. Endeavour Lions Park sign
8. Signature labelled Cooktown park bench
9. Anzac Park sign with white post and rail fence
10. Nature's Powerhouse suite of signage
11. Stone monument precast plaque signage

The strong design principles present within the design have enabled the Master Plan to develop clear, direct and equitable linking through the whole site and back to the heart of town centre. Nodal treatments have been illustrated to provide legibility in the landscape and inform the user that there is change in direction, or a particular destination has been reached. The width and suggested treatments of the paths have created hierarchy between the different journeys. These key design elements have created clarity of orientation for the public and visitors, as well as established solid foundations for an external wayfinding investigation to be completed.

In the interim, should information and interpretive signage be adopted or constructed by CSC, we endorse the existing signage type, as shown below.



4 The Concept Plan

4.1 Site Analysis and Context Plan

LEGEND & ITEMS OF SIGNIFICANCE

Environmental Considerations

Social Considerations

Economic Considerations

Cultural Considerations

Broad Area of Valuable Considerations

Nodes/Points of Interest of Valuable Considerations

Reclamation Area

Viewsheds

Existing Walking/Cycle Route

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Rail Shunting Yard Location and Rail Remnants

Granite Heritage Kerb, Railway Station and the Chinese Quarter

Reconciliation Rocks

Cooktown Well

James Cook Monument, 20 Pounder Cannon and The Australian Bicentennial National Trail Monument.

Aus. Volunteer Coast Guard Assoc. Inc. 295sqm.

James Cook Statue

Bicentennial Park

The Cairn, Milbi Wall and River of Life path.

The Queen's Steps, Mick the Miner and the Chinese Monument.

Fishermans Wharf Lease (660sqm) and Lease A - E

Wharfie's Run

Tree of Knowledge

Cooktown Blue Water Club Inc. Lease. 3450sqm.

Pilots Jetty

Reclamation Groin

Powder Magazine

Entry to Residential Subdivision

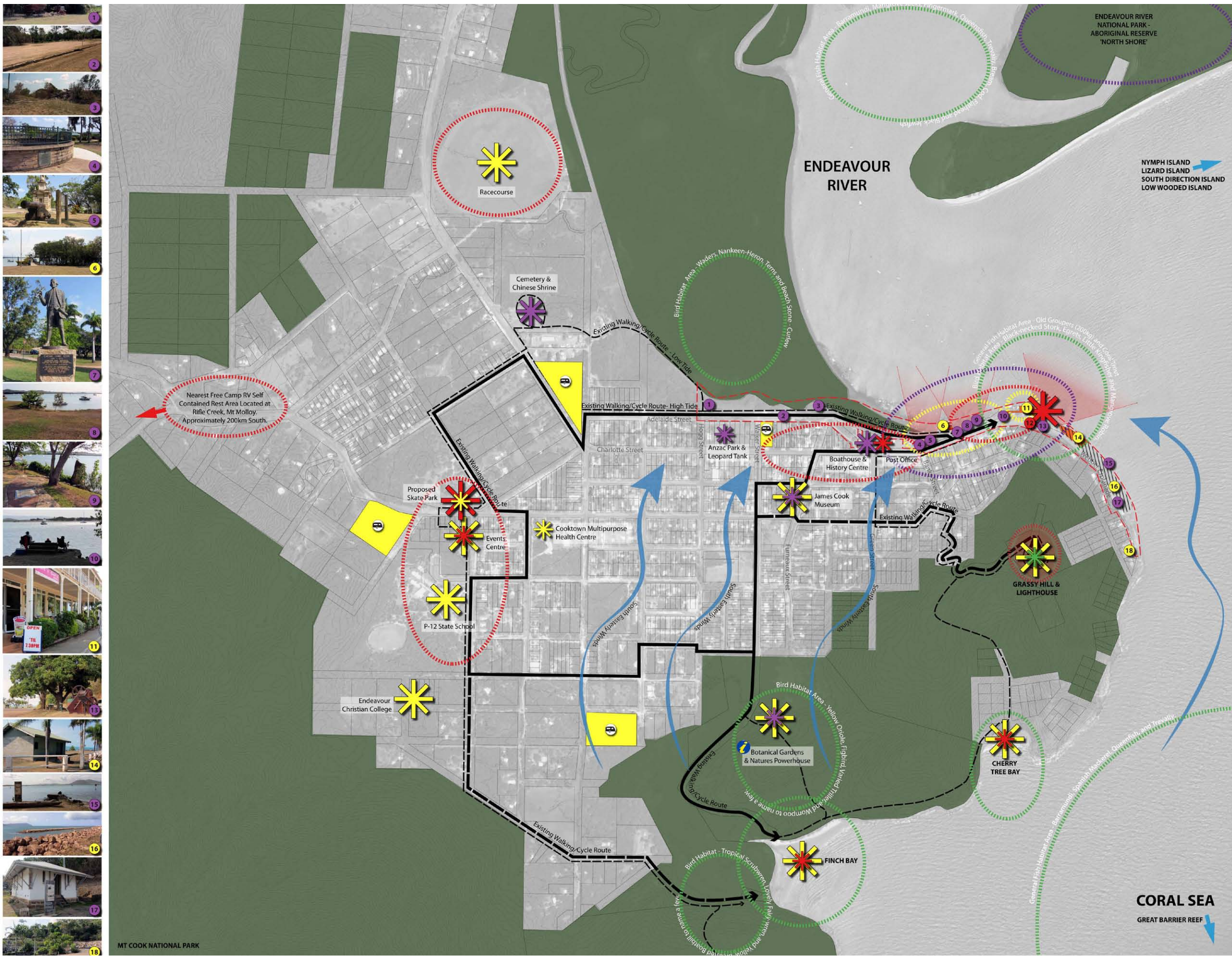
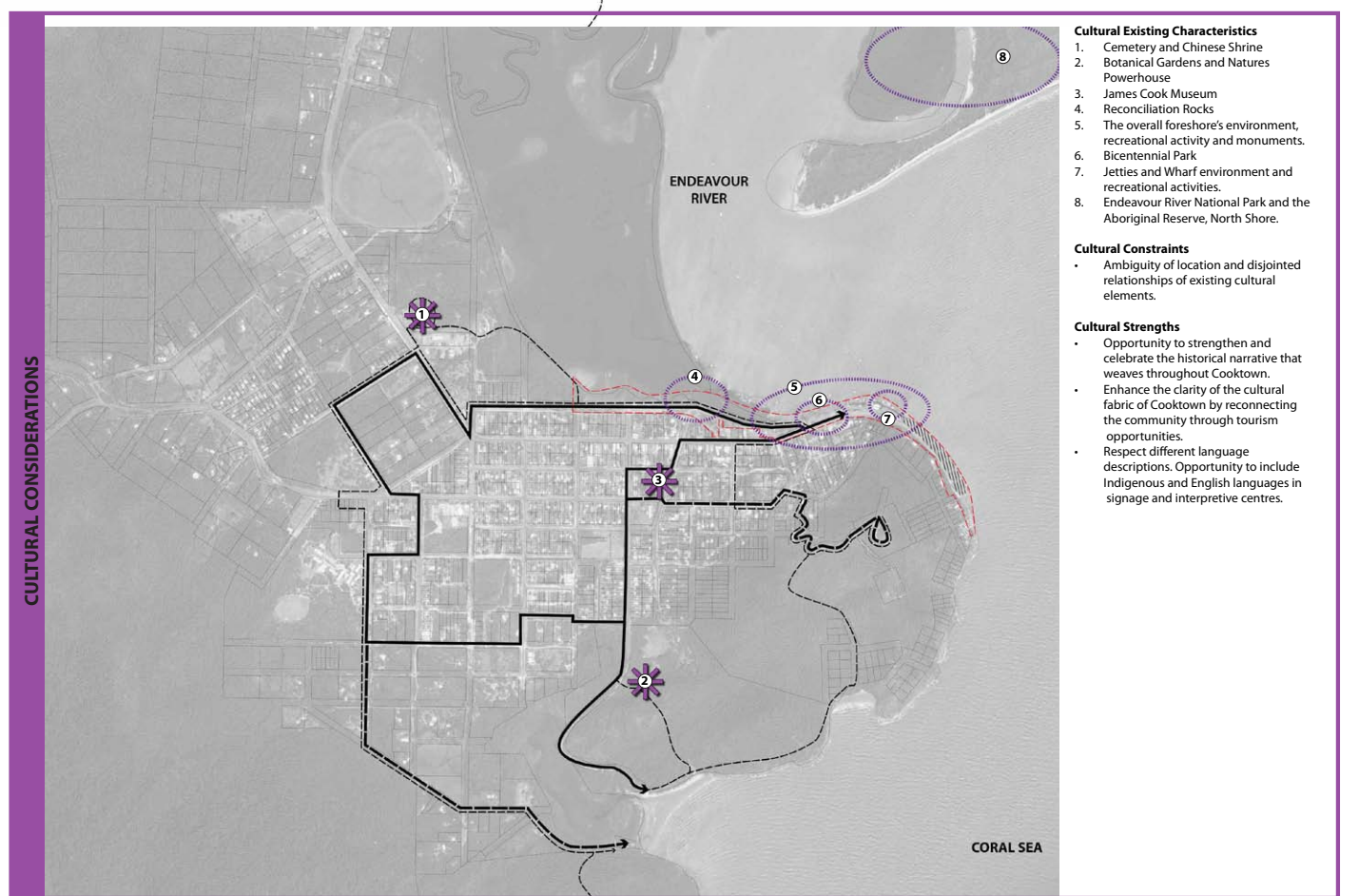
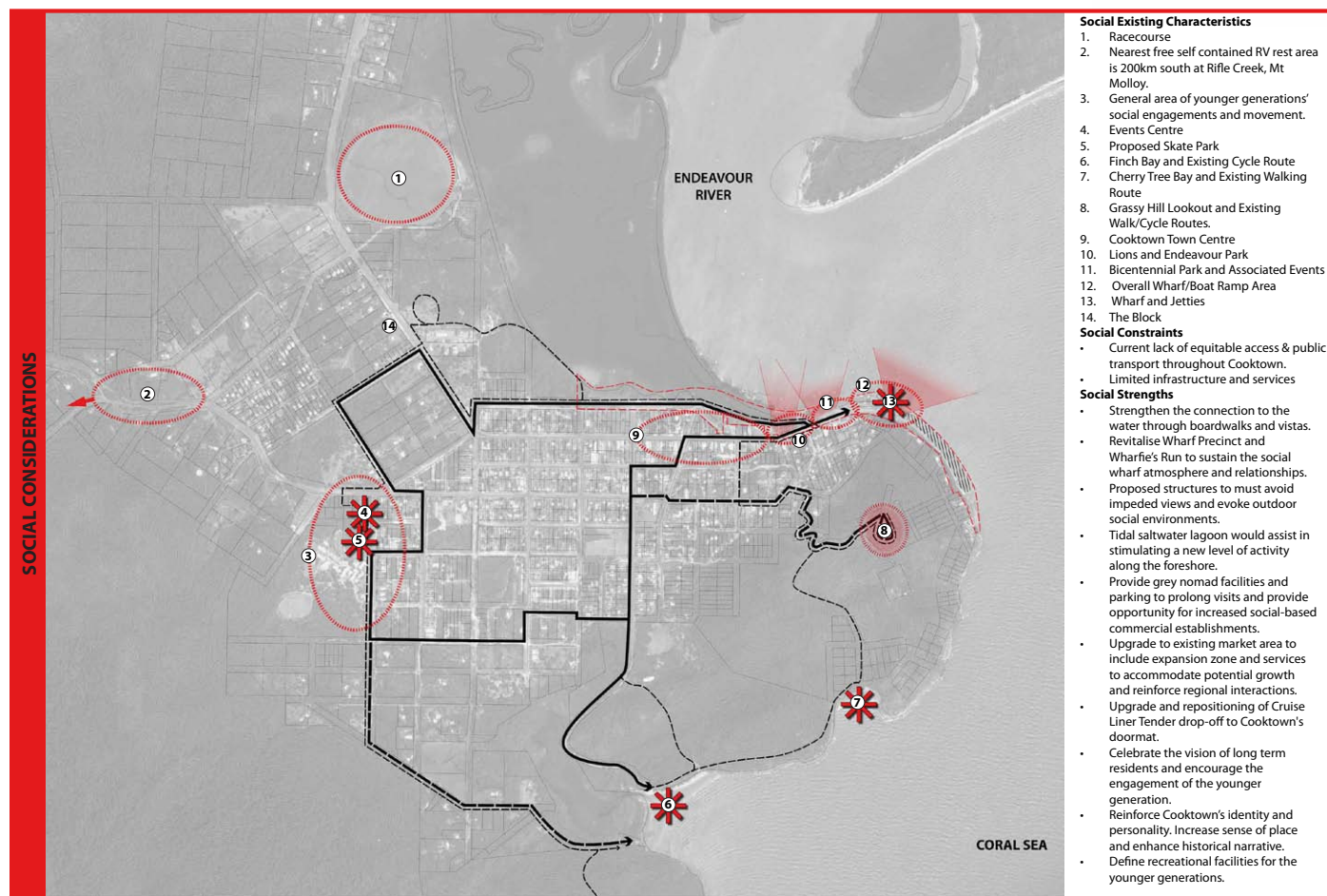
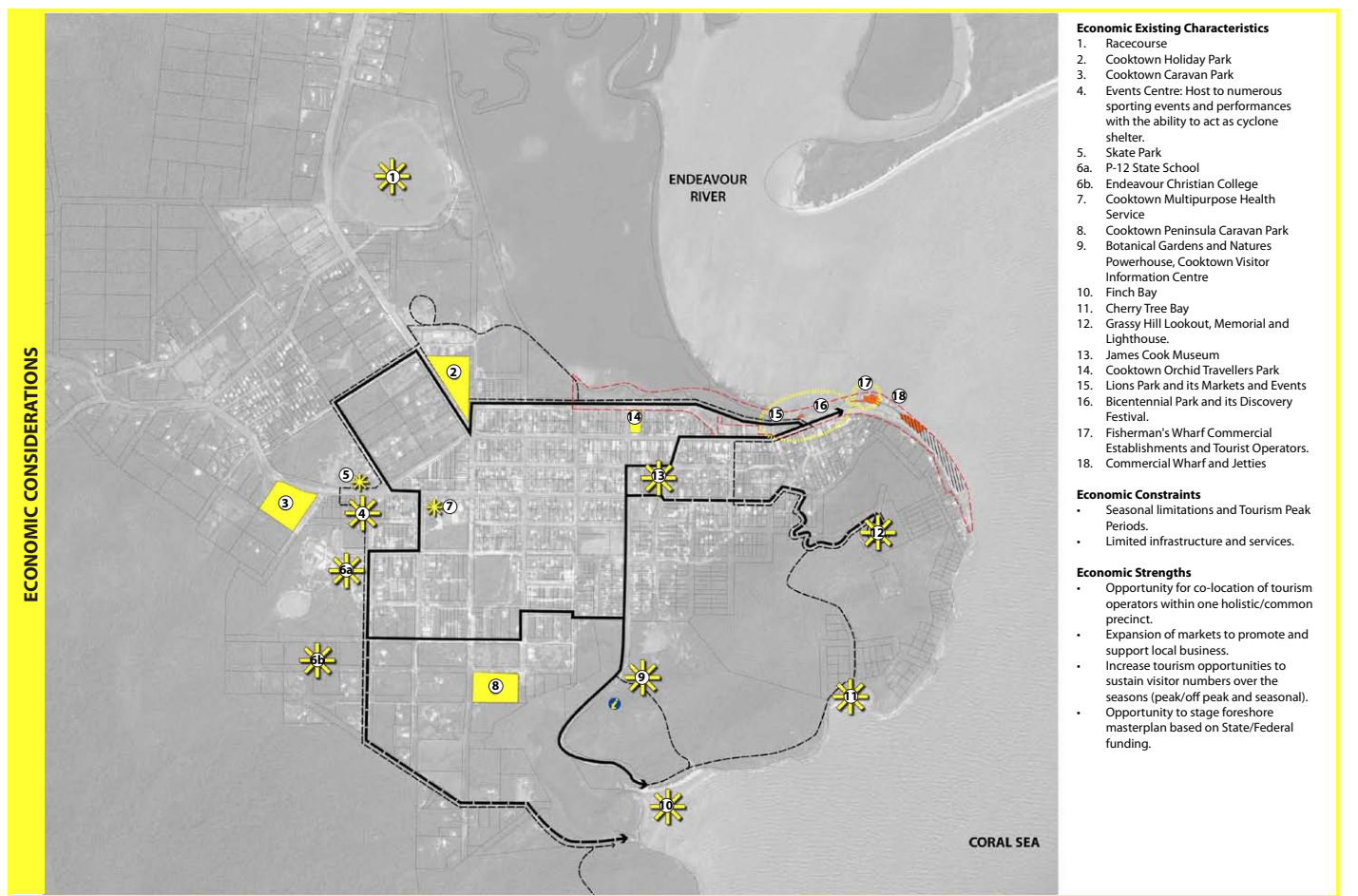
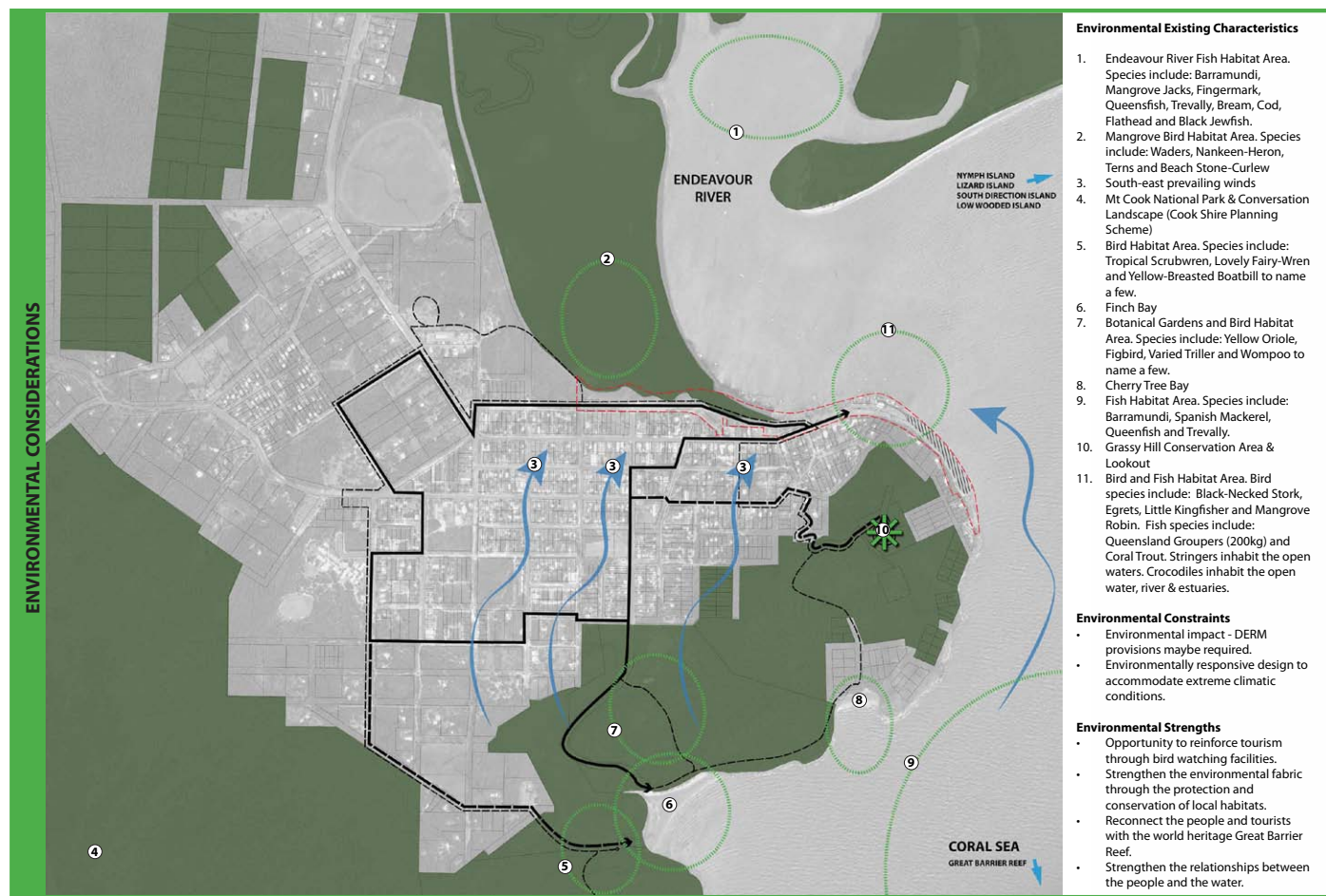


Figure – Site Analysis Plan (Page 1 of Concept Plan Package Issued to CSC November 2011)



4.2 Site Analysis

Figure – Opportunities & Constraints Plan (Page 2 of Concept Plan Package Issued to CSC November 2011)

4.3 Design Principles

The intent of the design principles is to envisage a cohesive network of features and attractions along the Endeavour River, that reflect the stories of Cooktown and encourage people, residents and visitors, to enjoy the ambience of the foreshore environs, and adjacent streets and neighbourhoods.

This set of guiding principles has been developed to assist, frame and act as filters for the design of the Master Plan and the associated infrastructure strategies, cost planning and implementation plan. The four principles are:

4.3.1 Principle One: Foreshore Anchors

By utilising existing heritage elements of the Powder Magazine to the north end and the Rail Shunting Yard to the south, the extent of the Cooktown Foreshore Precinct is clearly defined.

4.3.2 Principle Two: Foreshore Linkages

The inclusion of a tidal saltwater lagoon to the north of the foreshore assists in linking the existing Webber Esplanade across the entire site through clear, direct and equitable shared pathways.

4.3.3 Principle Three: Equidistant Placement

Redefining visual, pedestrian/cycle and vehicular connections along the foreshore through the identified key interpretive nodes and points of interest (monuments included), will sustain the user's experience along the entire length and strengthen the identity of Cooktown.

4.3.4 Principle Four: Precincts

The legibility of the foreshore is redefined by introducing four key precincts. These precincts are:

- Passive Natural Recreation Precinct
- Celebration Precinct
- Commercial Precinct
- Activity Precinct; and
- Potential Future Marina Precinct

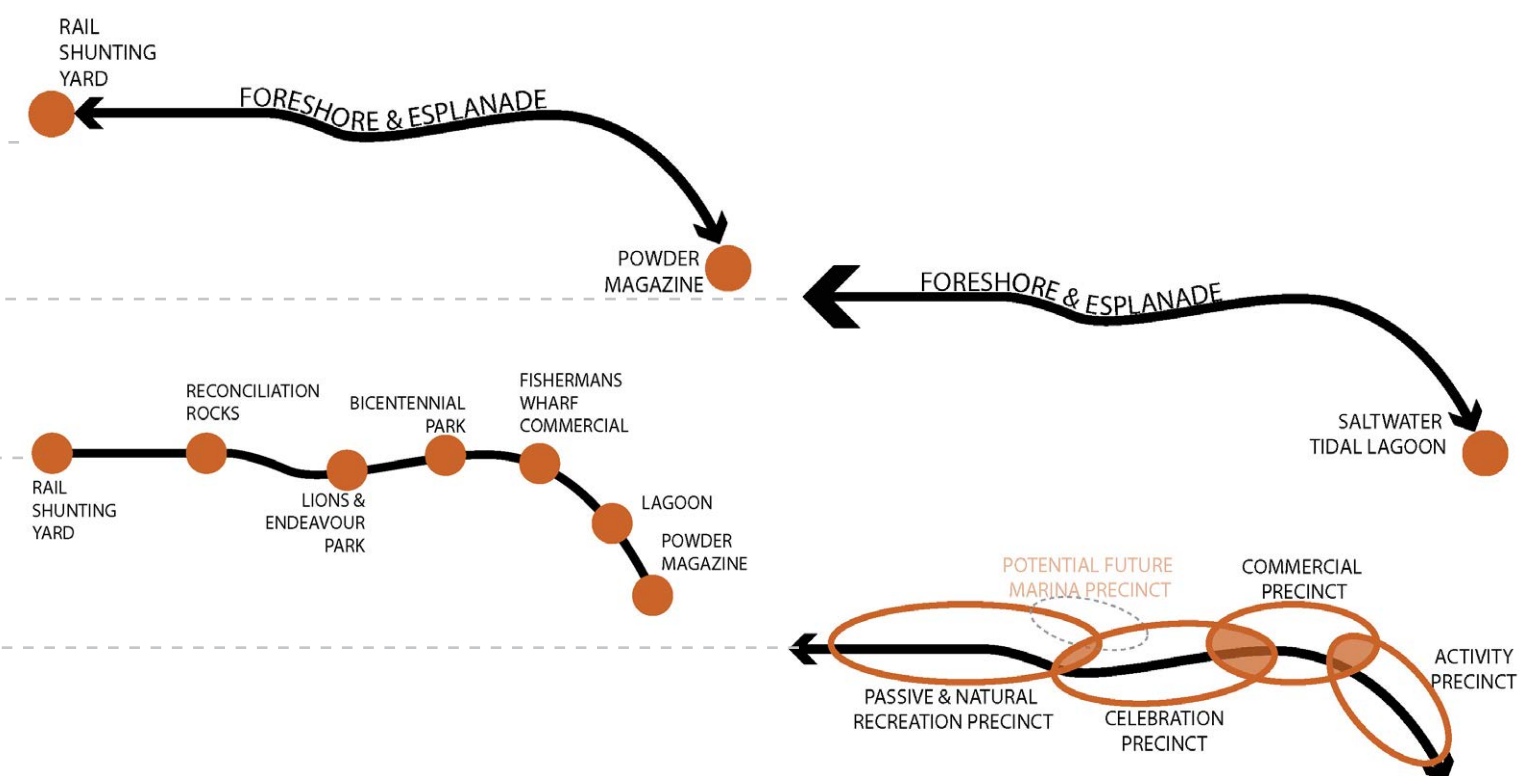
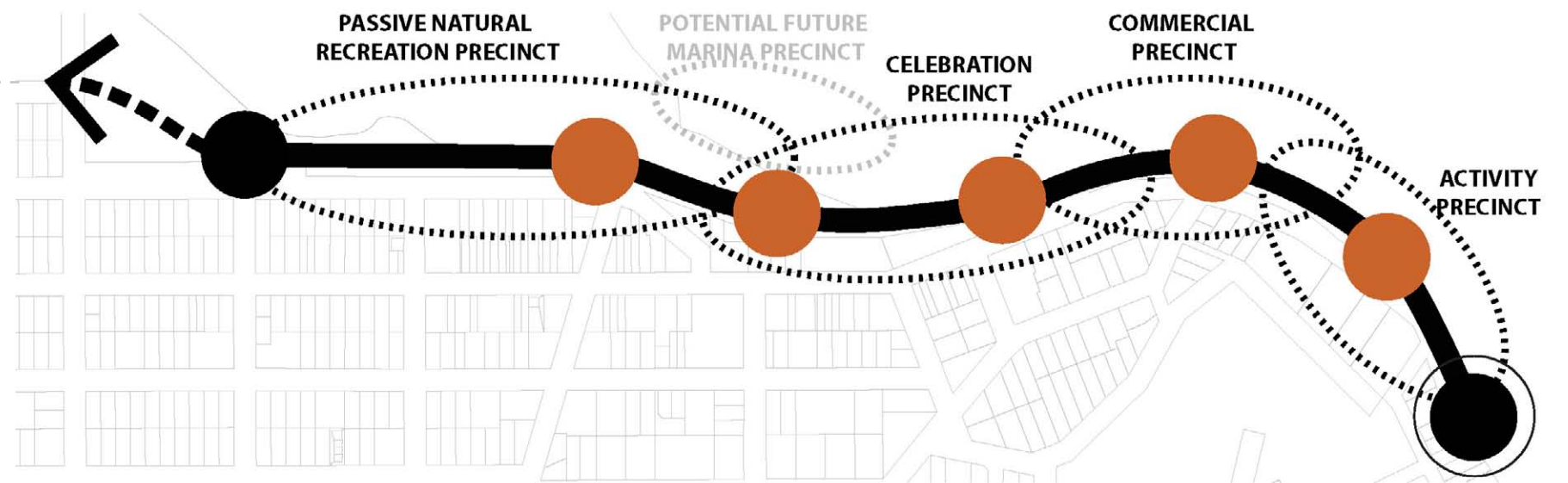


Figure – Design Principles

4.4 The Big Idea

These four design principles are the key drivers to ensure the sustainability and strength of this comprehensive and cohesive Foreshore and Esplanade Redevelopment, as shown in the above diagram. These design principles will establish 'The Big Idea' of reconnecting the residents and locals with the waterfront to revitalise the town. In addition, revitalise the town through private investment, on the back of public investment in the foreshore.



4.5 The Concept Plan & Precincts

The four precincts along the foreshore will help create function, activity and opportunity. Each precinct and its function can be described as:

4.5.1 Passive Natural Recreation Precinct

Set amongst dense mangroves and parallel to the town centre, this precinct provides both locals and tourists alike with a slower paced, relaxed and uniquely Cooktown recreational experience.

4.5.2 Celebration Precinct

This precinct unites the Region's residents and tourists through frequent community events and annual festivals, that celebrate Cooktown's rich history and vibrant future.

4.5.3 Commercial Precinct

The Commercial Precinct is the social heart of the waterfront and is supported by the existing and proposed commercial establishments, which will in return sustain Cooktown's economic future.

4.5.4 Activity Precinct

This precinct is the centre of all things active for all ages. Whether walking, riding, fishing, swimming or playing, this Activity Precinct is the outdoor energetic pulse of Cooktown.

4.5.5 Potential Future Marina Precinct

Based upon previous investigations and commissions - Cooktown Foreshore Management Plan 1998 - this area has been identified as the most environmentally and climatically suitable location for a potential future marina to be located. The Master Plan illustrated in this report has envisaged the possibility of a marina in this location and has responded by allowing the landscape design, connectivity and infrastructure to evolve if needed.

Letter codes on the plan -

a. Mangrove Boardwalk


b. Captain Cook Interpretive Centre

c. Upgraded Community Path


d. Reclaimed Openspace with retaining wall, path and reclamation wall

e. Shared path positioned on top of reclamation wall


Interpretive Node 1: Cooktown to Laura Rail Shunting Yard Interpretive Centre.

 Opportunity for an open and sheltered structure that graphically represents Cooktown's rail history, incorporates the historical rail remnants and acts as an initial point of information for the grey nomads/tourists. This interpretive centre would become the southern most attraction of the foreshore and would not need to be staffed. Equitable access required.


Interpretive Node 2: Reconciliation Rocks Interpretive Centre.

 Opportunity for an open and sheltered structure that graphically represents/re-enacts the story between Captain Cook and the Guugu Yimithirr people. This centre does not need to be staffed. Potential to enhance the landscape to complement the rocks and proposed interpretive centre. Consultation with local Indigenous community is required. The existing road corridor to be upgraded to maintain access for delivery/service vehicles. Equitable access required.


Interpretive Node 3: Lions & Endeavour Park Revitalisation

 Opportunity to upgrade the existing infrastructure, services and toilet facilities for events/markets with potential inclusion of service bollards. Retain the existing vehicle corridor by creating a shared path of adequate width and structural strength to double as informal vehicle access. Potential for removable bollards to limit vehicle access on non event/market days. Restoration of the area around the Captain Cook Memorial, Town Well and 20 Pounder Cannon to emphasise their importance within Cooktown's history. Lastly, preserve views to river/ocean and mature existing trees. Equitable access required.

Interpretive Node 4: Bicentennial Park

 Opportunity for non-staffed interpretive centre that graphically represents/re-enacts the story of Captain Cook's discovery. Centre could potentially be an open sheltered structure that does not impede views. Main shared path to travel along Charlotte Street behind grass sloped area. Opportunity to exaggerate existing grass sloped area to create outdoor amphitheatre. Investigation into the rotation of the Milbi Wall to enable clear connectivity. Consultation with local indigenous people is necessary. Upgrade of existing infrastructure and services for events/festivals with the potential inclusion of service bollards. Lastly, maintain existing views to river/open water. Equitable access required.

Interpretive Node 5: Powder Magazine Parkland

 As the northern most attraction of the Foreshore Redevelopment, this poses the potential to revitalise the area around the Powder Magazine, reinstate the views into and from the heritage listed structure with the inclusion of interpretive signage to illustrate its history. The inclusion of fishing pontoons (offset from the shared path) along the reclamation wall will improve the deep water fishing opportunities that are unique to Cooktown. The inclusion of proposed sheltered barbeques and picnic facilities will encourage locals and tourists alike to lengthen the duration of their visits. Equitable access required.

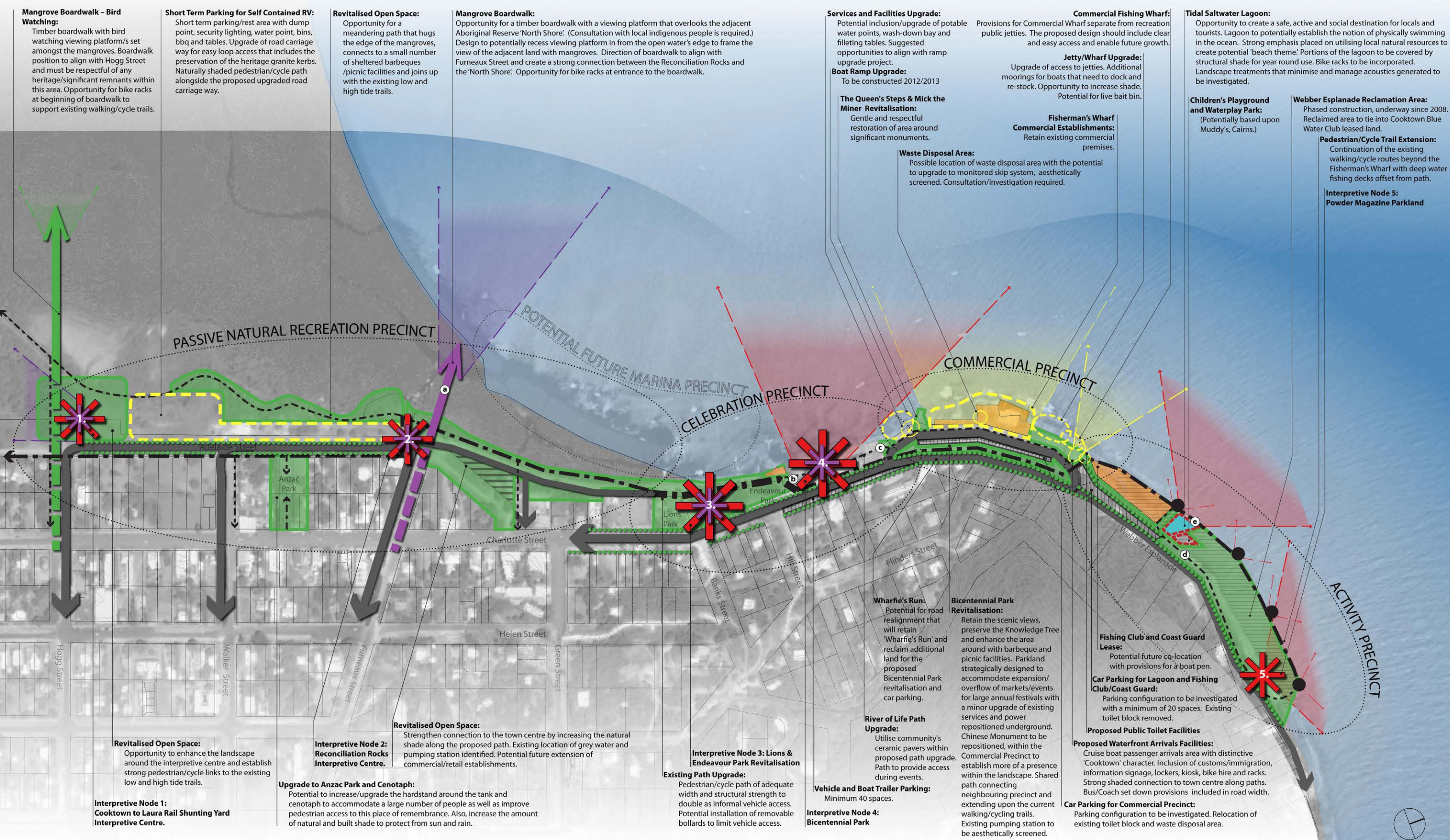


Figure – The Concept Plan (Issued to Cook Shire Council on November 2011)

4.6 Passive Natural Recreational Precinct

4.6.1 Key Benefits

- Increased and improved designated pedestrian and vehicle connectivity from town and along foreshore.
- Opportunity for more tourists (ie Grey Nomads) to lengthen their visit and further support the retail/commercial establishments in town.
- Cooktown would be further promoted as a user-friendly and accommodating tourist destination with facilities and attractions to meet everyone's needs.

Indicative images illustrating potential suggested design intent -

1. Timber Boardwalk into mangrove and tidal areas
2. Cenotaph & War Memorial with increased hardstand and access
3. Secondary meandering path
4. Interpretive Centre
5. Preservation and celebration of natural environment
6. RV Short Term Parking & Facilities (Recently Completed Facilities in Yelgun, NSW)
7. Shaded parking with designated pedestrian/cycle access



Figure – Passive Natural Recreation Precinct Indicative Images. Illustrating the potential form and function of the proposed projects, rather than the finishes and scale.

Cooktown Foreshore & Webber Esplanade Revitalisation

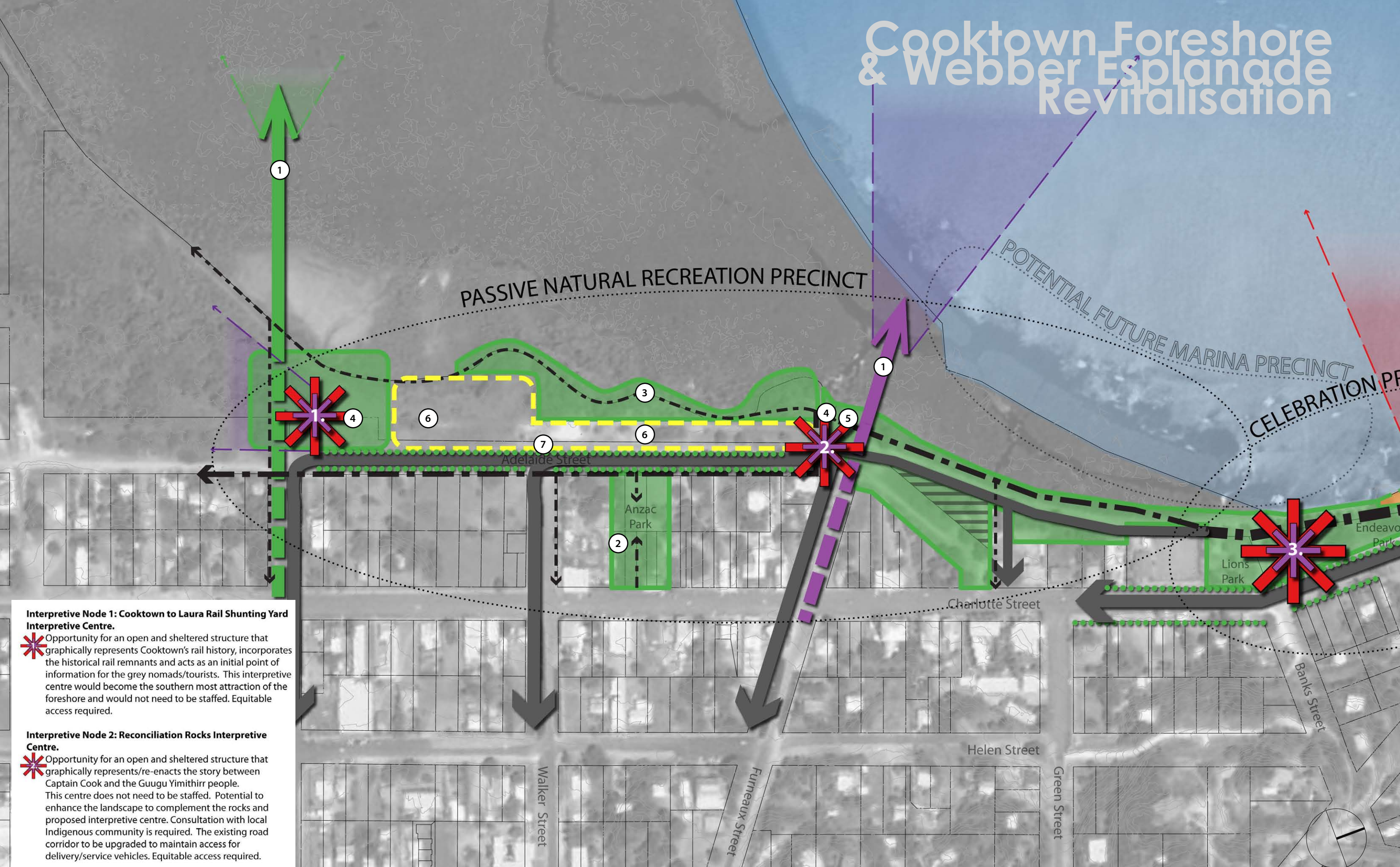


Figure – Passive Natural Recreation Precinct Plan

4.7 Celebration Precinct

4.7.1 Key Benefits

- Unites the region’s residents and attracts tourists through the organised celebration of history.
- Reinforces economic diversity through the proposed revitalisation of the market and event space.
- Preservation and celebration of the existing monuments would reinforce the identity of Cooktown.

Indicative images illustrating potential suggested design intent -

1. Park furniture with mosaic tiles - opportunity to include community pavers instead
2. Path upgrade with community pavers inserted
3. Formalised memorial area with open hardstand, clear access and recognition of significant monuments
4. Highlighting of monuments and significant artefacts
5. Streetscape revitalisation with designated parking, additional natural shade, power underground, wide pedestrian equitable paths and landscaping
6. Gently terraced grassed amphitheatre for outdoor events.



Interpretive Node 3: Lions & Endeavour Park Revitalisation

✱ Opportunity to upgrade the existing infrastructure, services and toilet facilities for events/markets with potential inclusion of service bollards. Retain the existing vehicle corridor by creating a shared path of adequate width and structural strength to double as informal vehicle access. Potential for removable bollards to limit vehicle access on non event/market days. Restoration of the area around the Captain Cook Memorial, Town Well and 20 Pounder Cannon to emphasise their importance within Cooktown's history. Lastly, preserve views to river/ocean and mature existing trees. Equitable access required.

Interpretive Node 4: Bicentennial Park

✱ Opportunity for non-staffed interpretive centre that graphically represents/re-enacts the story of Captain Cook's discovery. Centre could potentially be an open sheltered structure that does not impede views. Main shared path to travel along Charlotte Street behind grass sloped area. Opportunity to exaggerate existing grass sloped area to create outdoor amphitheatre. Investigation into the rotation of the Milbi Wall to enable clear connectivity. Consultation with local indigenous people is necessary. Upgrade of existing infrastructure and services for events/festivals with the potential inclusion of service bollards. Lastly, maintain existing views to river/open water. Equitable access required.



Figure – Celebration Precinct Indicative Images. Illustrating the potential form and function of the proposed projects, rather than the finishes and scale.



Figure – Celebration Precinct Plan

4.8 Commercial Precinct

4.8.1 Key Benefits

- Traffic management and standardised parking.
- Revitalised commercial area with proposed additional establishments.
- Upgrade of recreational uses, facilities and services.

Indicative images illustrating potential suggested design intent -

1. Traffic Management and Designated Car Parks
2. Upgrade to existing jetties with separation of public/recreational and commercial fishermen
3. Upgrade to paths and retain viewsheds
4. Naturally shaded shared path offset from road corridor
5. Possible retrofitting opportunities
6. Strong separation between pedestrians and vehicles around commercial outlets, as well as designated bus bay

Interpretive Node 4: Bicentennial Park

✳ Opportunity for non-staffed interpretive centre that graphically represents/re-enacts the story of Captain Cook's discovery. Centre could potentially be an open sheltered structure that does not impede views. Main shared path to travel along Charlotte Street behind grass sloped area. Opportunity to exaggerate existing grass sloped area to create outdoor amphitheatre. Investigation into the rotation of the Milbi Wall to enable clear connectivity. Consultation with local indigenous people is necessary. Upgrade of existing infrastructure and services for events/festivals with the potential inclusion of service bollards. Lastly, maintain existing views to river/open water. Equitable access required.

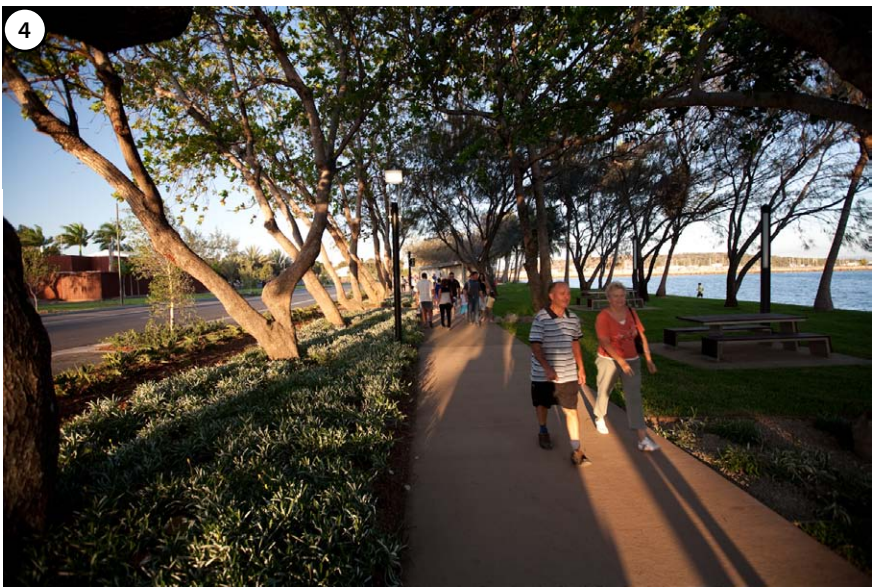


Figure – Commercial Precinct Indicative Images.
Illustrating the potential form and function of the proposed projects, rather than the finishes and scale.

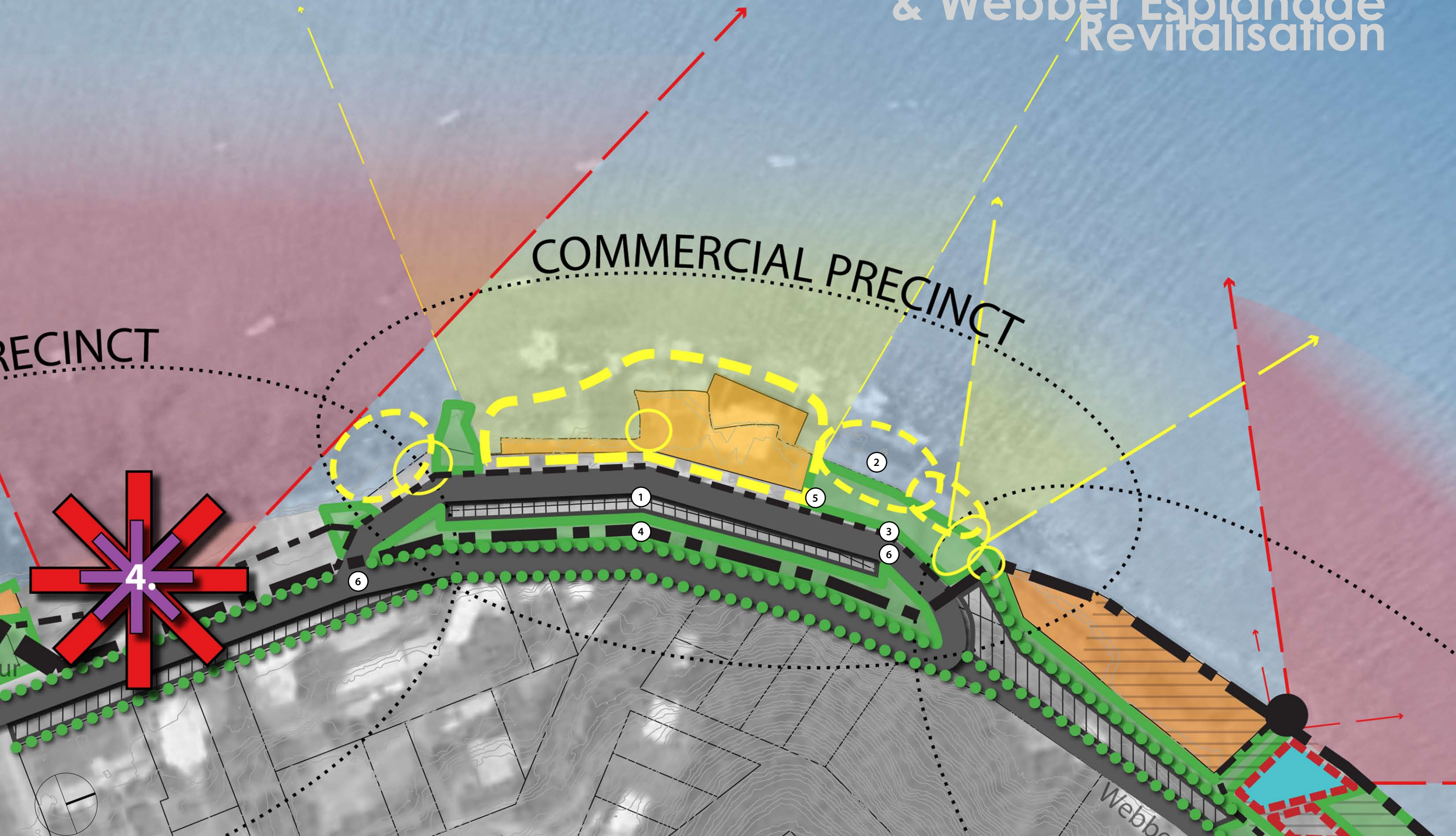


Figure – Commercial Precinct Plan

4.9 Activity Precinct

4.9.1 Key Benefits

- Lagoon will be a regional attraction. Potential for nominal size of 25m lap pool with additional area for wading, beach and waterplay adjoining.
- Supports neighbouring Commercial Precinct.
- Promotes an active and social lifestyle for residents of all demographics.

Indicative images illustrating potential suggested design intent -

1. Wynnum Saltwater Pool
2. Muddy's Playground
3. Children's Coastal Playground
4. Groin Shared Path
5. Sheltered Picnic and Barbeque Facilities
6. Lookout over water



Interpretive Node 5: Powder Magazine Parkland
As the northern most attraction of the Foreshore Redevelopment, this poses the potential to revitalise the area around the Powder Magazine, reinstate the views into and from the heritage listed structure with the inclusion of interpretive signage to illustrate its history. The inclusion of fishing pontoons (offset from the shared path) along the reclamation wall will improve the deep water fishing opportunities that are unique to Cooktown. The inclusion of proposed sheltered barbeques and picnic facilities will encourage locals and tourists alike to lengthen the duration of their visits. Equitable access required.

Figure – Activity Precinct Indicative Images.
Illustrating the potential form and function of the proposed projects, rather than the finishes and scale.

Cooktown Foreshore & Webber Esplanade Revitalisation

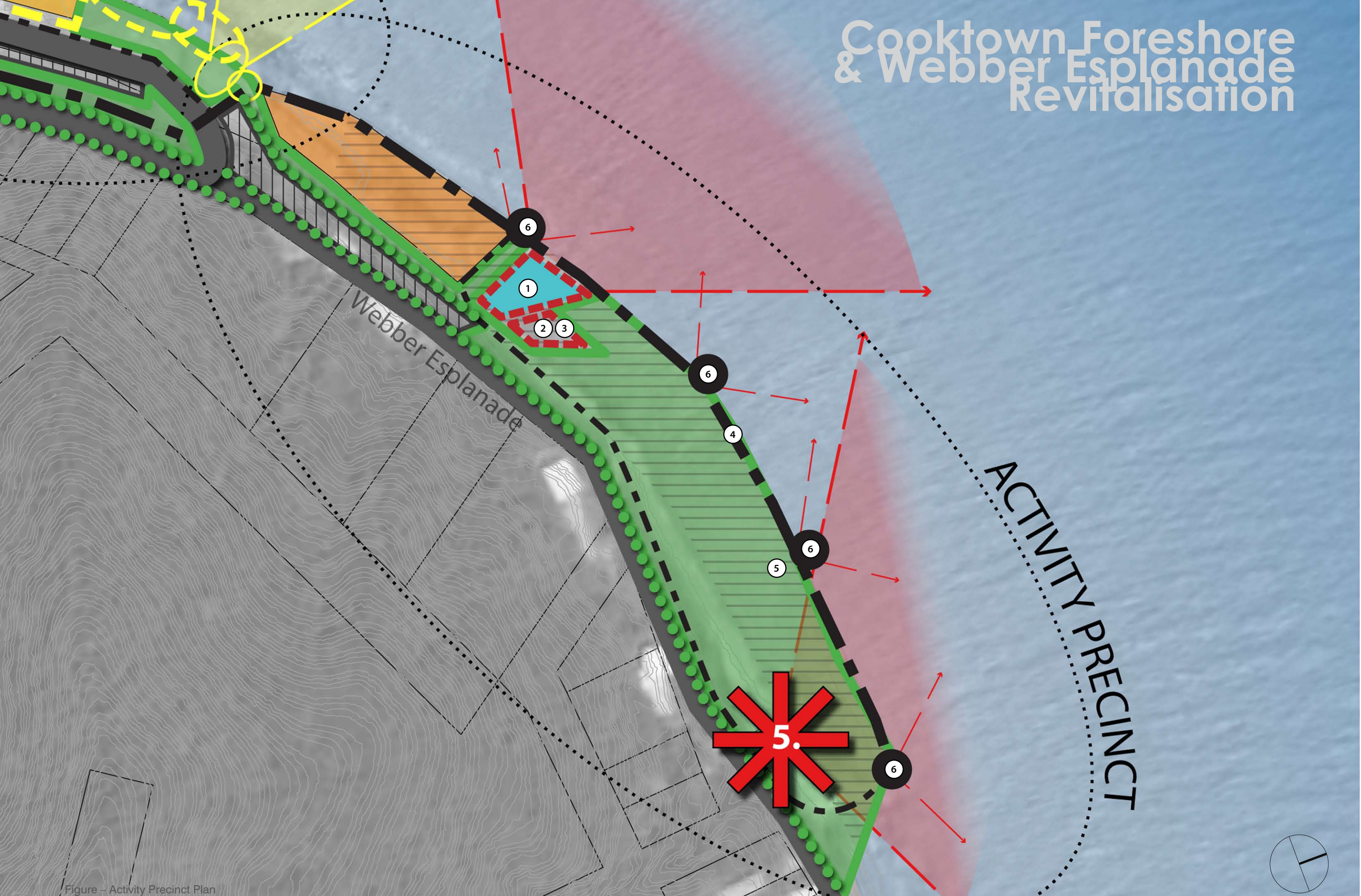


Figure – Activity Precinct Plan

5 The Master Plan

5.1 The Master Plan - The Holistic Vision

The purpose of a Master Plan is to outline long-term projects or functions of a site, council or development. It illustrates a series of steps to be carried out and goals to be accomplished. A Master Plan is a long term strategy that takes:

- A sustainable approach to the destination’s development;
- Focuses on key precincts and the associated social, environmental, cultural and economic opportunities;
- Builds on existing strengths and leverages off established assists;
- Revitalises and drives significant competitive improvement and positioning; and
- Represents the destination’s cultural, social, environmental and economic existing and potential values.

CSC, in consultation with the community and the mentioned stakeholders, have identified a need for and commitment to the revitalisation of the foreshore. The study area, defined by CSC, is from Hogg Street, along Adelaide Street, terminating at the Powder Magazine located at the northern end of Webber Esplanade.

CSC commissioned Tract Consultants to prepare the Cooktown Foreshore and Webber Esplanade Master Plan as part of Council’s preparations to seek funding. This Master Plan serves as a key reflection of Council’s consultation process. It is essential to endorse and include community feedback and input on the development options for the waterfront and the adjacent streets and land uses.

The purpose of this Cooktown Foreshore and Webber Esplanade Master Plan is to illustrate the range of uses and infrastructure upgrades that consultation and CSC have identified as able to attract residents and visitors into the water corridor and, in doing so, reconnect the community with the mighty Endeavour River and all it has to offer including its historical, social, cultural, recreational and environmental values.

This foreshore Master Plan identifies a number of distinct precincts along the waterfront and proposes a variety of nodes within these precincts. A proposed network of pedestrian and cycle paths links the precincts and nodes via wide and equitable shaded corridors.

In considering options for the revitalisation of the water frontage and adjacent land uses, the following opportunities and constraints must be considered. (Please note these opportunities and constraints were also identified with the Concept Plan documentation).

5.1.1 Master Plan Opportunities

- Opportunity to reinforce tourism through bird watching facilities.
- Strengthen the environmental fabric through the protection and conservation of local habitats.
- Reconnect the people and visitors with the world heritage Great Barrier Reef.
- Strengthen the relationships between the people and the water.
- Opportunity for co-location of tourism/business operators within one holistic/common precinct.
- Expansion of markets to promote and support local business and the community.
- Increase tourism opportunities to sustain visitor numbers over the seasons (peak/off peak and seasonal).
- Strengthen the connection to the water through boardwalks, fishing platforms and vistas.
- Revitalise Wharf Precinct and Wharfie’s Run to sustain the social wharf atmosphere and relationships.
- Proposed structures must avoid impeding views and evoke outdoor social environments.
- Tidal saltwater lagoon and waterplay park would assist in stimulating a new level of activity along the foreshore.
- Provide RV tourists facilities and short term parking to prolong visits that will directly increase the demand for hospitality, retail, eco-tours and accommodation. This will stimulate the economic opportunities for the local community.
- Upgrade to existing market area to include expansion zone and services to accommodate potential growth and reinforce regional interactions.
- Upgrade and reposition the Cruise Liner Tender drop-off to Cooktown’s doormat.
- Celebrate the vision of long-term residents and encourage the engagement of the younger generation.
- Reinforce Cooktown’s identity and personality. Increase sense of place and enhance historical narrative.
- Define recreational facilities for the younger generations.

5.1.2 Master Plan Constraints

- Environmental impact - DERM provisions may be required
- Environmentally responsive design to accommodate extreme climatic conditions
- Seasonal limitations and tourism peak periods
- Limited infrastructure and services
- Low maintenance infrastructure; and
- Current lack of access, paths and links throughout Cooktown for people using wheelchairs and other mobility devices.

5.1.3 The Master Plan Objectives, Strategies and Aims / Actions:

The Master Plan vision has clearly established the design intent and purpose for this investigation. In order for this Master Plan to fulfil its desired purpose, objectives, strategies and aims have been identified to ensure its long-term successful delivery. Four areas have been identified in order to clearly address the identified opportunities and constraints that face the CSC. These areas are:





































1. Distinctively Cooktown;
2. Accessibility and Connectivity;
3. Economic Diversity; and
4. Quality of Livability and Experience.

Objectives	Strategies	Aims / Actions
1. Distinctly Cooktown, the Gateway of the Cape York		
1. Develop Cooktown as a distinctive place in Tropical North Queensland.	a. Endorse the Foreshore as the place for a classic, iconic ‘Cooktown Experience’. b. Consolidate Cooktown Foreshore as an operational wharf and gateway to Endeavour River and Great Barrier Reef.	Develop, upgrade and revitalise foreshore activities along Endeavour River to establish the foreshore as the place to/for: <ul style="list-style-type: none"> Riparian and open water fishing, deep water on-shore fishing and reef snorkelling; Water based tourism, ie. crocodile sight seeing and fishing charters; Hospitality, retail and commercial venues, as well as Entrance Arrivals Terminal for cruise boat operators; Jetty and wharf facilities; Walking and cycle trails; Swimming in secure waters; and Large iconic events and markets. Upgrade current wharf and jetty facilities to establish a safe and operational environment for both the recreational and professional fishermen. Retain the requirement for vehicle access to the operational wharf. Opportunity for upgrade to accommodate growth of industry, services and tourism.
2. Increase market and tourism awareness of Cooktown	a. Continue to evolve, implement and promote Cooktown’s brand, ‘The history is just the beginning’, in a clear and consistent narrative with focus on the foreshore. b. Cultivate a domestic and international product that increases the awareness of Cooktown as a must-see destination and a livable town.	Overlay Cooktown’s current brand to ensure/implement: <ul style="list-style-type: none"> Communication strategy to develop awareness and delivery of foreshore development; Revisit the marketing plan to best sell Cooktown and its proposed foreshore revitalisation; and Generate a resource / marketing initiative targeted at tourism operators and foreshore hospitality/retail establishments to better strengthen the ‘Cooktown Experience’ strategy. Leverage off Cooktown’s accessibility by road, air and sea; proximity to Port Douglas and Cairns, Great Barrier Reef and Cape York Peninsula; rich cultural history; and significant Australian history.
3. Establish the Endeavour River as one of the underpinning assets of Cooktown and its foreshore.	a. Preserve, protect and restore natural flora and fauna ecosystems.	<ul style="list-style-type: none"> Preserve the healthy ecosystems by implementing sensitive foreshore design and management. Promote the importance of the existing mangrove communities and their unique ecosystems through boardwalks, interpretive signage and connectivity. Protect the waterway to restore riparian ecosystems.
4. Continue to build upon the whole region’s attractions, like Grassy Hill and Cape York Peninsula.	a. Improve connectivity, access and promotion of the broader regional attractions by co-ordinating with council and tourism bodies. Develop and improve the affiliation and understanding of the broader regional attractions.	To aid this strategy, the following is recommended: <ul style="list-style-type: none"> Upgrade, improve and unify visitor information, and interpretive and directional signage to ensure that the iconic Cooktown and Tropical Far North Queensland attractions display market appeal, interest and significance; Promote the existing local recreational activities, such as fishing, boating, bird watching and outdoor activities; Promote the existing regional recreational activities such as mountain bike riding, cycling, horse back riding, four wheel driving and hiking; Promote the local social, environmental and cultural attractions, such as monuments and statues, Rail Shunting Yard, Reconciliation Rocks, Endeavour Park, Lions Park and Mangroves to name a few; Promote the regional social, environmental and cultural attractions, such as Great Barrier Reef, Cape York Peninsula, Nature’s Powerhouse, Botanical Gardens, Grassy Hill, Cherry Tree bay, Finch Bay, James Cook Museum, Cemetery and Chinese Shrine, Boathouse and History Centre, to name a few; Encourage the development of low impact/key/scale and high yielding commercial nature-based tourism opportunities for Cooktown and regional surrounds; Improve access and connectivity along the foreshore and create the opportunity to extend and upgrade upon existing trails/walks within the town and/or region; and Within the main foreshore open spaces, provide facilities such as picnic shelters, barbecues, signage, rubbish bins, bike racks, lookouts and boardwalks to amplify the user experience and increase the length of visit within the foreshore.
5. Strengthen Indigenous, European and Asian cultural tourism	a. Maintain and develop festivals, markets and events. b. In relation to the foreshore and Endeavour River, maintain and develop sports related events. c. Revitalise cultural tourism, in particular indigenous, along the foreshore and utilise the opportunity for economic growth amongst the community.	To aid these strategies, the following is recommended: <ul style="list-style-type: none"> Retain, advance and promote, as resources allow, existing festivals that utilise the foreshore, ie Discovery Festival; Create community based events, as resources allow, such as outdoor movie nights, twilight performances and art installations; Potential to review events schedule/calendar to ensure profitability, value and leverage are maximised for Cooktown’s stakeholders; and Implement a programme that can evolve with the community by incorporating their creativity through installations, ie community art pavers. Implement sporting events, as resources allow, that utilise the foreshore through management and planning. Investigate, liaise and consult with local indigenous and other cultural communities to clarify their vision for cultural tourism operations; Promote indigenous arts, cultural interpretation and storytelling within the foreshore area; and Preservation and protection of significant sites, ie Reconciliation Rocks.
6. Establish the foreshore as a key tourism attraction that connects and promotes the region.	a. The Foreshore	To aid this strategy, the following is recommended: <ul style="list-style-type: none"> Investigate and upgrade the wharf and jetty infrastructure to accommodate growth in the industry and popularity of recreational fishing; Provide pedestrian and cycle equitable paths along the entire foreshore that extend and connect to existing walking trails to accommodate those using mobility equipment; Maintain the deepwater access to ensure the operations of the wharf and attractiveness to the recreational sailor/yachties; Establish an Entrance Arrivals Kiosk on the foreshore to create a lasting first impression on visitors; Revitalise the sense of arrival (by road) into Cooktown, through streetscaping, clear directional signage and framing of vistas; Investigate the opportunity for a marina to be developed with associated mixed commercial, retail and hospitality outlets; Retain viewsheds and vistas from existing roads (that border the foreshore), Wharfie’s Run and pedestrian/cycle paths; Increase and upgrade access points to water’s edge to re-establish the unique character and experience of Cooktown; Retail artefacts, landforms, monuments and vegetation of significance to preserve existing character and beauty of Cooktown’s foreshore; Opportunity to increase hospitality establishments and increase the social culture along foreshore; Reconnect the foreshore with the town centre to stimulate the useability and accessibility to the water’s edge and economical opportunities; Focus upon the revitalisation of the foreshore from Lions Park to Webber Esplanade; Opportunity to integrate community and visitor facilities and infrastructure within the foreshore; Preserve the existing mature vegetation within the foreshore area and open up the vistas with careful pruning and canopying lifting; and Implement traffic management solutions to upgrade road reserve, provide more vehicle and trailer parking, clearly designate pedestrian crossing areas and maintain sweeping vistas to the Endeavour River.

Objectives	Strategies	Aims / Actions
2. Accessibility and Connectivity		
1. Advance regional and local access to the Foreshore.	<ul style="list-style-type: none"> a. Attract visitors and residents off primary and secondary road networks into the foreshore. b. Identify and promote Cooktown regionally and nationally as a vital part of the marine, maritime and fisheries industry. c. Revitalise the foreshore and connecting roads to establish a clear designated and identifiable public recreational area for the community and visitors. d. Upgrade the existing paths to create a continuous equitable pedestrian and cycle network connecting to and within the foreshore development. e. Increase the types and amount of water access points along the foreshore. f. Investigate and endorse a marina development. g. Improve visual connection to the foreshore and the water's edge. h. Identify and illustrate information and history about elements within the foreshore. i. Consider possible future sea level rise and flooding within the design perimeters. 	<p>To aid these strategies, the following is recommended:</p> <ul style="list-style-type: none"> • Ensure that well signed and clear turnoffs are maintained and/or upgrade to enable easy access to the foreshore and its facilities. Improve streetscaping treatment along streets and roads that enter or connect to the proposed foreshore to create hierarchy within the journey and clear indication of reaching a destination. • Work with Government bodies and stakeholders to establish and upgrade the harbour, wharf and marina facilities to provide the appropriate facilities and infrastructure to re-establish Cooktown as an operational and growing wharf, that is vital to the Tropical North Queensland economic network. • Increase and organise public water access through traffic management, boat ramp upgrade, path network, open space, framed vistas and various types of access points; • Provide equitable stairs, ramps and pontoons for all abilities to reach the water's edge; • Provide safe and secure aquatic recreational environments to deliver another experience for the community and visitors, ie Saltwater Tidal Lagoon and Water Play Park; and • Make the foreshore a distinctive 'public' area through signage, maintenance and accessibility to increase community pride, ownership and investment. • Provide continuous shaded waterfront paths that connect and link the precincts, interpretive centres/nodes, boardwalks and openspaces; • Establish strong pedestrian and cycle connection between the foreshore and the town centre; and • Provide equitable access and appropriate infrastructure to amenities, facilities and attractions for all abilities. • Upgrade boat ramp and wharf/jetty facilities to accommodate growth of use and operations. Provide easy embark/disembark movements, as well as restricted vehicle access and main services; • Provide fishing pontoons to increase deepwater fishing opportunities; and • Provide areas of direct water access to small tinnies, canoes and kayaks for soft launching and landing sites. • Review 'Tropical North Queensland: Tourism Opportunity Plan (2010-2020)' for sustainable development opportunities and funding towards Cooktown Port and Marina. Investigate potential position, infrastructure and land based facilities. Unique facilities that may be required are fuel and pump outlets, water and electricity, drop off and tender facilities, as well as short-term mooring. • Maintain, open and frame views to the water from the roads, paths, commercial establishments and open spaces. Allow for vehicle pull over areas, designated Wharfie's Run, pedestrian rest areas with shade and bench seating and low set structures. • Establish a clear suite of signage that illustrates information, directions, history and other unique stories that promote the value and significance of Cooktown, its foreshore and the Endeavour River. • Identify potential impacts, restrictions and constraints of the possibility of sea level rise over a prolonged time span. Design and plan commercial establishments, infrastructure and open space to adapt to the possibility.
2. Improve movement, connectivity and access within and around the Foreshore.	<ul style="list-style-type: none"> a. Simplify the ability to navigate, wayfind and orientate within and around the foreshore. b. Promote non - vehicle movements by improving pedestrian and cycle paths / corridors / trails. c. Upgrade, replace or develop paths and connections that are suitable for all abilities and mobility devices. 	<p>To aid these strategies, the following is recommended:</p> <ul style="list-style-type: none"> • Simplify and upgrade existing wayfinding and directional signage to read as a clear co-ordinated suite that reinforces the identity and character of Cooktown; and • Create visual hierarchy within roads and paths to provide a sense of direction through streetscaping, lighting and types of trees and pavements. • Increase and upgrade pedestrian and cycle linkages to primary attractions and facilities; • Develop the opportunities to cycle around Cooktown and its foreshore with increased facilities such as bike hire, racks, lockers, drinking fountains, clear signage to attractions beyond, lighting and shade; • Opportunity for private investment to facilitate bike hire, tours and appropriate amenities; • Encourage and create opportunity for private investment/tourism operator to run guided walking and riding tours of the foreshore, Cooktown and Region. • Opportunity to develop upon self-guided walking and horse or bike riding tours through afore mentioned facilities and clear marketing/promotions. • Opportunity to develop equitable connections of adequate width and grade (to and along the foreshore) to support the aging population and tourism demographic.
3. Economic Diversity		
1. Develop Cooktown as a distinctive place in Tropical North Queensland.	<ul style="list-style-type: none"> a. Encourage stakeholders and private investors (primary tourism operators) to embrace the proposed economic opportunities within the foreshore area, by providing unique 'Cooktown experiences' through linking and networking services and attractions. 	<p>To aid this strategy, the following is recommended:</p> <ul style="list-style-type: none"> • Co-ordinate like or complementary services, experiences or attractions to offer the most memorable 'Cooktown Experience' for visitors; and • Inspire local businesses to offer package deals that may include accommodation, recreation, hospitality and tourism operations. Encourage commercial establishments, both existing and proposed, to cross-sell and package deal. This will enable local or regional commercial stakeholders the most efficient and comprehensive economic opportunity with diverse outcomes and solutions.
2. Establish the Endeavour River as one of the underpinning assets of Cooktown and its foreshore.	<ul style="list-style-type: none"> a. Identify foreshore, waterfront and river related economic, commercial and investment opportunities. 	<p>Investigate and evaluate the possible investment options:</p> <ul style="list-style-type: none"> • Mixed Use Commercial: cafe / restaurant / bars / retail / ice creamery; • Waterfront recreational establishments / venues: tavern, seafood restaurant, Fishing Club venue; • Sustainable eco-friendly 'green tourism': bike hire, walking / riding guided tours, marine tours, avitourism (bird watching); • Boat and equipment hire; and • Additional charters and cruises.
3. Nurture and stimulate the community and regional industry appointment and understanding of the values of Cooktown's Foreshore.	<ul style="list-style-type: none"> a. Meet the increasing consumer demands by establishing effective business partnerships with stakeholders, by sourcing funds and complying with planning schemes that back the tourism industry. 	<p>To aid this strategy, the following is recommended:</p> <ul style="list-style-type: none"> • Co-ordinate with national, state and regional tourism bodies to guarantee market information is updated, provided to and understood by stakeholders. Ensure websites and databases reflect the upgraded status of the foreshore tourism attractions and facilities. In particular, ensure that 'disabled/all abilities friendly' tourism rating is updated and advertised to increase the type and amount of visitors; and • Intensify the understanding of the master plan outcomes within the council and community to attract finance and government funding that best responds to Cooktown's investor needs.
4. Nurture and stimulate public and private investment.	<ul style="list-style-type: none"> a. Promote foreshore based commercial opportunities and tourism operations to the public and private parties. 	<p>To aid this strategy, the following is recommended:</p> <ul style="list-style-type: none"> • Encourage and endorse new private and public investment within the foreshore; • Identify economic and commercial opportunities, in keeping with Cooktown's unique character, that attract national and international investment, ie tender boat facilities that may attract more cruise companies; • Identify various government and private sources that financially support business development for both indigenous and non- indigenous, and encourage ongoing business management training, support and mentoring; • Reinforce Cooktown's marketing brand with existing and upcoming stakeholders and provide training and support to reinforce marketing brand and a sustainable economic goal; and • Generate an investment prospectus to highlight the main infrastructure constraints and opportunities, avenues for investment and possible partnership opportunities.

Objectives	Strategies	Aims / Actions
3. Economic Diversity - Continued		
5. Establish the foreshore as a key tourism and resident attraction that connects and promotes the region.	a. The Foreshore	<p>Some aims for implementation of economic diversity are:</p> <ul style="list-style-type: none"> Consult with the community and indigenous groups to create cultural and heritage tourism that locally and regionally supports the economy; Ensure that planning schemes, zoning and land uses are retained in order to preserve Cooktown’s iconic character, heritage, ecosystems and picturesque landscape, that are quintessential in economic growth; Explore the feasibility of an educational/training/storage establishment for community groups / interests that utilises existing buildings, by generating a Usability Report that identifies these opportunities; Explore and promote private investments in family orientated hospitality outlets and various retail shops such as arts, information kiosk, bait and tackle shop, general store and souvenir shops; Inspire existing commercial leaseholders to revitalise the current physical condition of establishments, and encourage a united working relationship with potential commercial establishments; Upgrade traffic management and flow to enable locals and tourists to move safely between vehicles and attractions/commercial establishments, have the ability to park in close proximity to their destination, and have a comfortable walk/ride to their destination; Consult and investigate the opportunity for a marina development that supports the operational wharf and existing marine based commercial establishments, incorporates dredging practises, provides the appropriate facilities and amenities, encourages short or long-term anchorage and provides opportunity for fishermen co-operative and additional commercial outlets; Investigate opportunities for the waterfront land to be designated for public use to reinforce the Cooktown character, identity and community pride; Incorporate a saltwater tidal lagoon to offer all the community and tourists with a year-round safe swimming environment, that plays on the beauty of the surrounding landscape through its beach theme; Incorporate water play park equipment to engage the younger generations and stimulate their growth and presence within the community. Consultation within this process will generate a sense of pride and ownership within the younger community, which will then directly reduce vandalism and damage; Incorporate self-contained RV short-term parking to enable visitors to safely park and replenish their vehicles. Parking to be within foreshore and in close proximity to the town centre to provide the opportunity to stretch their legs and experience Cooktown’s commercial establishments; Promote private investments in bike and boat hire, bus service, the interpretive media and mobile cart businesses; and Build upon existing weekly markets and annual events to maximise on and utilise all attributes of the Foreshore.
Objectives	Strategies	Aims / Actions
4. Quality of Livability and Experience		
1. Revitalise the appearance of the Foreshore.	<p>a. Create a sense of arrival when approaching the foreshore from road and water.</p> <p>b. Revitalise the appearance and presentation of existing and proposed commercial establishments.</p>	<p>To aid these strategies, the following is recommended:</p> <ul style="list-style-type: none"> Clear directional signage and streetscaping to create hierarchy of streets and paths. Encourage investment and responsibilty in the physical appearance and presentation of commercial establishments.
2. Revitalise the experience of the Foreshore.	a. Increase the duration that a resident or visitor utilises and experiences the foreshore. Increase the variety of recreational activities for all. Improve the environmental conditions to make the experience more pleasurable.	<p>To aid this strategy, the following is recommended:</p> <ul style="list-style-type: none"> Provide fishing pontoons off the reclamation wall to allow for additional deep water fishing opportunities; Increase the amount of natural and built shade at main points of interest along the foreshore; Increase the amount of bench seating, picnic and barbeque shelters to reactivate family or social group orientated activities; Promote a clean, environmentally conscious foreshore with adequate rubbish bins, water points and solar initiatives where possible; Emphasise the relationship to water’s edge and breathtaking vistas; Provide various recreational activities and equipment for all ages and abilities, ie swimming lagoon, exercise equipment, waterplay park, pathways, boardwalks, boating and bird watching; Make the foreshore easily and pleasantly accessible by road and water, ie water arrivals kiosk; and Establish the foreshore as very accessible by vehicle and foot through appropriate traffic management and planning, allocated parking and defined clear paths of access.
3. Manage and monitor development and growth to ensure the distinctive Cooktown character, heritage and values are retained yet advance and align with market expectations.	a. Review land uses and planning provisions to support development and tourism opportunities, yet respect Cooktown’s overall branding and appeal.	<p>To aid this strategy, the following is recommended:</p> <ul style="list-style-type: none"> Orientate future development towards the water’s edge to unify the engagement with the foreshore and amplify the experience; Guarantee that future development measures are retained and modified to best suit Cooktown’s desired approach, ie restricted building heights, viewsheds, character and environmental standards; Maintain balance between waterfront public open space and commercial development opportunities to retain the unique experience and engagement which the Cooktown community have with the water’s edge; Undertake survey mapping and audit of the foreshore project area, public assets (monuments), public art, infrastructure and existing vegetation; and Ensure the foreshore development responds to Highest Astronomical Tide (HAT), Lowest Astronomical Tide (LAT), climatic conditions, riparian and ocean ecosystems, sustainable fishing practices, heritage register and urban growth.
4. Illustrate Cooktown’s history and information into the Foreshore.	a. Develop a dynamic and clear integrated method for portraying Cooktown and illustrating the attractions, culture and experiences.	<p>Some implementation aims to obtaining an overall placemaking approach are:</p> <ul style="list-style-type: none"> Create a dynamic, distinctly Cooktown signage suite that represents and communicates the rich history, culture, directions and information within the foreshore and region; Include interpretive centres / signage to portray the stories, history and adventures; Upgrade self guided or guided walking/riding tours along the foreshore and beyond, to increase the experience and appreciation for the town; Increase accessibility to maps, brochures and information centres; and Events, festivals and markets.

LEGEND

-  200L TREE
-  100L TREE - PATH TREE
-  45L TREE - STREET TREE
-  45L TREE - CLUSTERED TREE
-  PALM TREE
-  EXISTING TREE -
Exclusion & Arborist Review
-  GROUNDCOVER STANDARD
GC-S(4x140mm /m2)
-  GROUNDCOVER PREMIUM (GC-P)
(2x200mm /m2 + 3x140mm /m2)
-  GROUNDCOVER REHABILITATION
GC-R (1xVIROTUBE /m2)
-  MULCH RING
(Beneath Existing Mature Trees)
-  PT1: Coloured Insitu Concrete:
Sandblast Finish, Stamped,
125mm Depth, reinforced.
-  PT2: Natural Grey Insitu Concrete:
125mm Depth, reinforced,
-  PT2 - VEHICLE GRADE
Natural Grey Insitu Concrete:
180mm Depth, reinforced,
-  PT3: Coloured Insitu Concrete:
125mm Depth, reinforced,
-  PT3 - VEHICLE GRADE
Coloured Insitu Concrete:
180mm Depth, reinforced,
-  DECOMPOSED GRANITE: DECO
75mm deep
-  SOFTFALL
(Colours & characters to match existing)
-  TIMBER BOARDWALK / DECK
-  EXISTING MONUMENT
-  INTERPRETIVE CENTRE*
- Proprietary Item
-  SHELTER - Pitched Roof*
3.2m wide x 3.8m long x 3.6m high
-  SHELTER WITH BBQ*
(1 Elec. Plate with Cabinet)
-  SHELTER WITH BBQ*
(2 Elec. Plates & Cabinets)
-  FISH CLEANING SHELTER
WITH BBQ*
(Bench and water point)
-  UNISEX TOILET *
(1 x Disabled cubicle & 1 x
standard cubicle)
-  PARK FURNITURE SET
(Match existing CSC range)
- All Abilities Version Avail.
-  BENCH SEAT
(Match CSC existing range)
-  BIKE RACK
(1 Set = 4 Racks)
-  REMOVABLE BOLLARD
(Timber or S/Steel)
-  SERVICE BOLLARD
(Water and Electricity)
-  RUBBISH BIN
(Match existing CSC range)
-  DRINKING FOUNTAIN
(Match existing CSC range)
-  WATER POINT / TAP
-  BEACH SHOWER
- Potable water timed shower
-  POOL FENCE & GATE
- In accordance with regulations
-  BATTEN VERTICAL SCREEN
- Installed around waste disposal
& existing service
-  STREET LIGHT
- Pole Height 5m
-  UPLIGHTS
- Monuments & Key Items
-  FLUORESCENT LIGHTS
- Shelters
-  SITE BOUNDARY
-  H.A.T. LINE
(Approx. 1.72m)

* Symbol colour does not represent suggested finish colour/s.



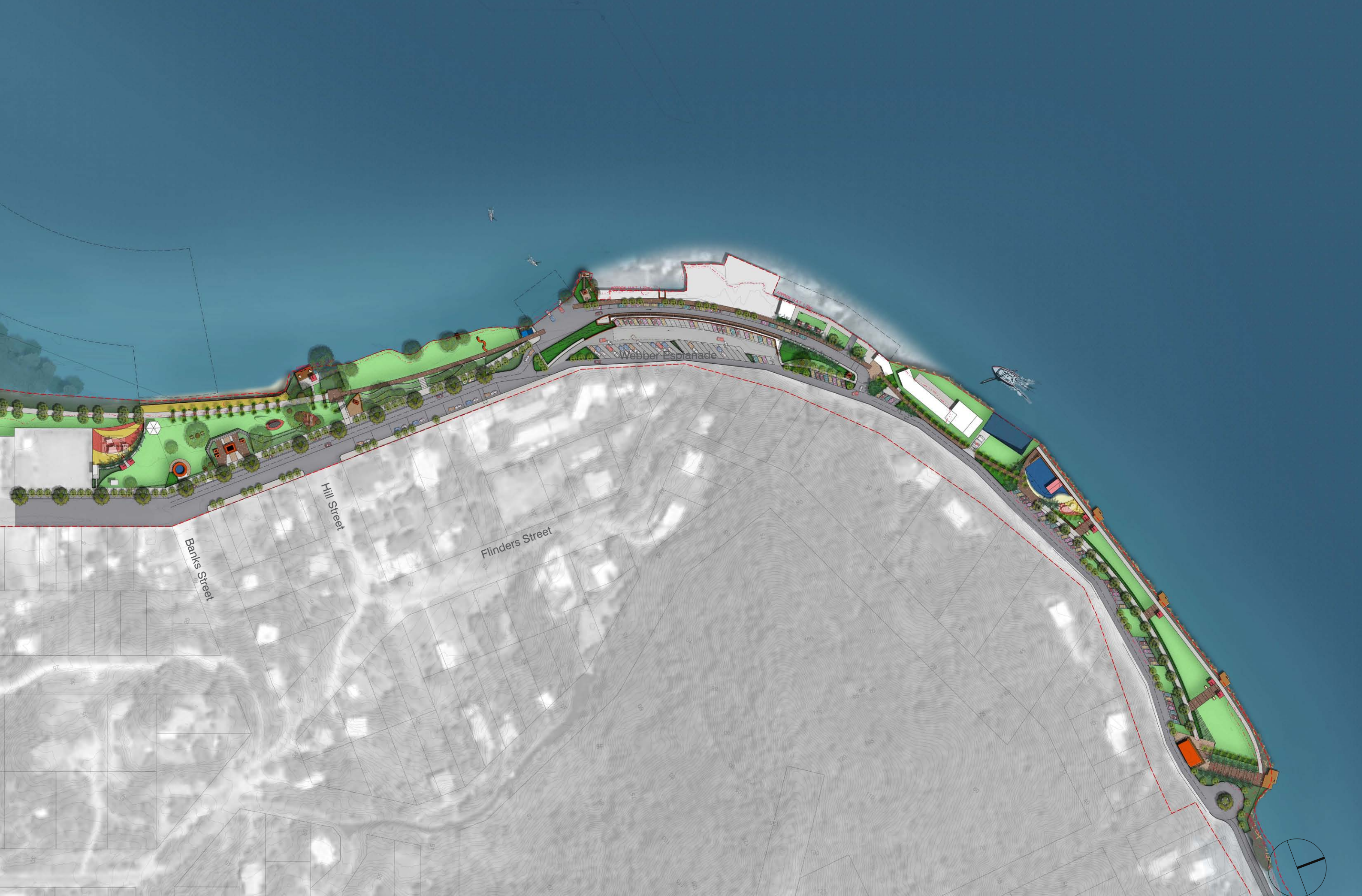


Figure – Scale NTS - Cooktown Foreshore & Webber Esplanade Master Plan

5.2 Hardscape & Furniture Palette



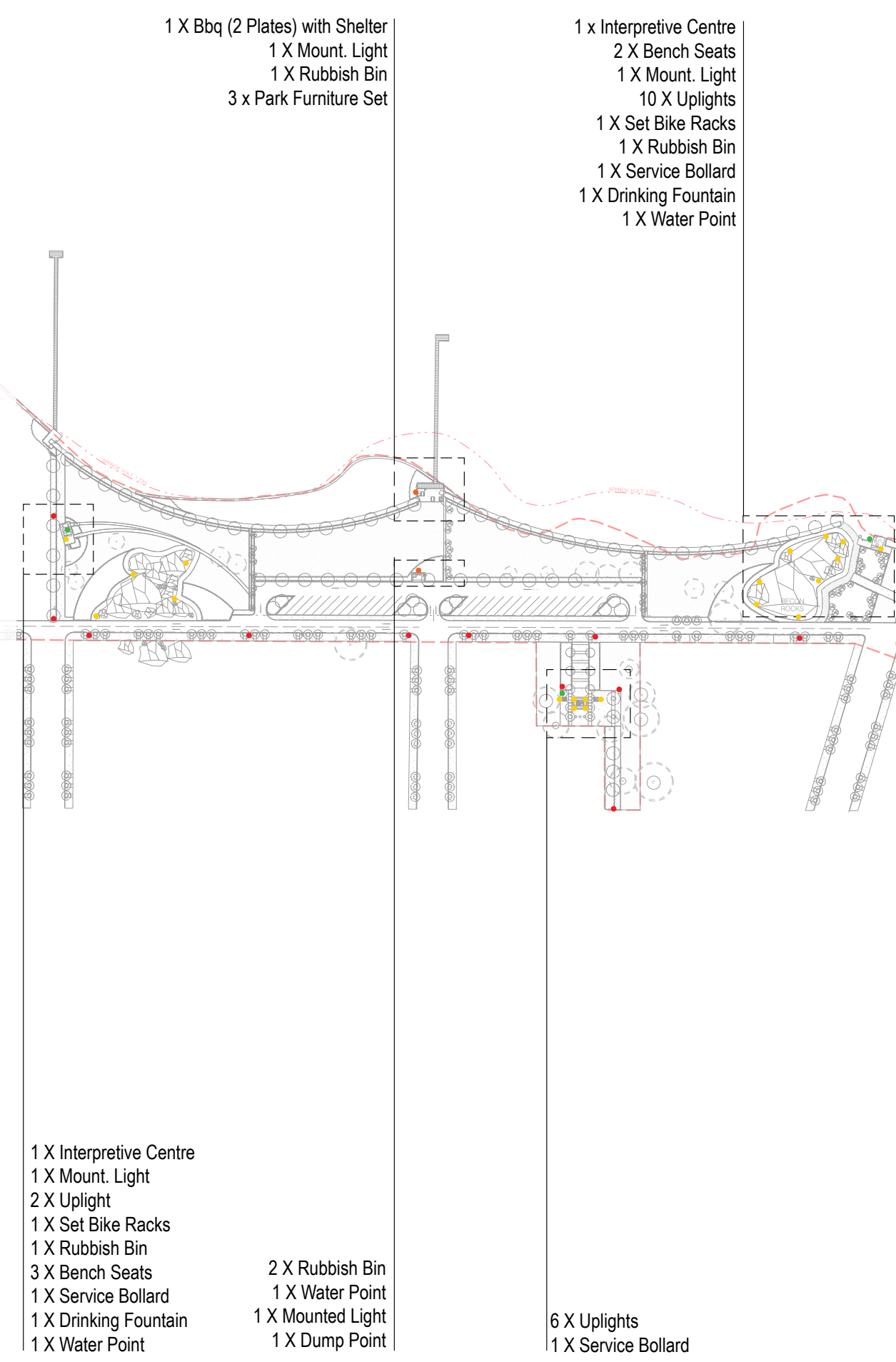
Legend

- SOFTFALL (Colours & characters to match existing)
- TIMBER BOARDWALK / DECK
- EXISTING MONUMENT
- INTERPRETIVE CENTRE* - Proprietary Item
- SHELTER - Pitched Roof* 3.2m wide x 3.8m long x 3.6m high
- SHELTER WITH BBQ* (1 Elec. Plate with Cabinet)
- SHELTER WITH BBQ* (2 Elec. Plates & Cabinets)
- FISH CLEANING SHELTER WITH BBQ* (Bench and water point)
- UNISEX TOILET * (1 x Disabled cubicle & 1 x standard cubicle)
- PARK FURNITURE SET (Match existing CSC range) - All Abilities Version Avail.
- BENCH SEAT (Match CSC existing range)
- BIKE RACK (1 Set = 4 Racks)
- REMOVABLE BOLLARD (Timber or S/Steel)
- SERVICE BOLLARD (Water and Electricity)
- RUBBISH BIN (Match existing CSC range)
- DRINKING FOUNTAIN (Match existing CSC range)
- WATER POINT / TAP
- BEACH SHOWER - Potable water timed shower
- POOL FENCE & GATE - In occurrence with regulations
- BATTEN VERTICAL SCREEN - Installed around waste disposal & existing service
- STREET LIGHT - Pole Height 5m
- UPLIGHTS - Monuments & Key Items
- FLUORESCENT LIGHTS - Shelters
- SITE BOUNDARY
- H.A.T. LINE (Approx. 1.72m)

* Symbol colour does not represent suggested finish colour/s.

Shopping List- Furniture

- 03 Shelters**
Proprietary item that can be installed by Council. Roof structure able to be lowered during severe weather conditions. Shelter dimensions 3.2m wide x 3.8m length x 3.6m high.
- 08 BBQ Shelters (1 Plate & 1 Cabinet)**
Proprietary item to compliment CSC range. Electric barbeque on concrete slab base. Mounted light required. Shelter dimensions 3.2m wide x 3.8m length x 3.6m high.
- 01 BBQ Shelters (2 Plate & 2 Cabinet)**
Proprietary item to compliment CSC range. Electric barbeque on concrete slab base. Mounted light required. Shelter dimensions 3.2m wide x 3.8m length x 3.6m high.
- 01 Fishing Cleaning Shelter**
Proprietary item with water point, bench and lighting. Dimensions 2.5m wide x 2.2m length x 2.1m high.
- 21 Park Furniture Sets**
Proprietary item to match CSC range. Timber wheelchair design available.
- 33 Bench Seats**
Proprietary item to match CSC range.
- 19 Rubbish Bins**
Proprietary item to match CSC range.
- 08 Drinking Fountains**
Proprietary item to match CSC range.
- 08 Water Points**
Tap facility at main functional areas.
- 01 Beach Shower**
Proprietary item to match CSC range.
- 10 Bike Racks**
Proprietary item to match CSC range. 4 Racks per set.
- 09 Removable Bollards**
Proprietary item. Timber or metal optional.
- 09 Service Bollards**
Water and electricity provisions
- 02 Interpretive Centres**
Proprietary item to match CSC furniture and colours. Lighting required. Non-staffed.
- 01 Interpretive Centres - Custom**
Custom Endeavour Boat shape / form design for illustrate Captain James Cook history. Mounted and uplighting required. Non-staffed.
- 06 Outdoor Exercise Equipment**
Proprietary item. 2 stations / 3 pieces per station. Softfall base beneath equipment.
- 12 Water play Park Equipment**
Proprietary items using reticulated saltwater. Softfall base. 3 zone design for toddlers, teenagers and family. 3 or 4 pieces per zone.



- 1 X Bbq (2 Plates) with Shelter
- 1 X Mount. Light
- 1 X Rubbish Bin
- 3 x Park Furniture Set

- 1 x Interpretive Centre
- 2 X Bench Seats
- 1 X Mount. Light
- 10 X Uplights
- 1 X Set Bike Racks
- 1 X Rubbish Bin
- 1 X Service Bollard
- 1 X Drinking Fountain
- 1 X Water Point

- 1 X Interpretive Centre
- 1 X Mount. Light
- 2 X Uplight
- 1 X Set Bike Racks
- 1 X Rubbish Bin
- 3 X Bench Seats
- 1 X Service Bollard
- 1 X Drinking Fountain
- 1 X Water Point
- 2 X Rubbish Bin
- 1 X Water Point
- 1 X Mounted Light
- 1 X Dump Point

- 6 X Uplights
- 1 X Service Bollard

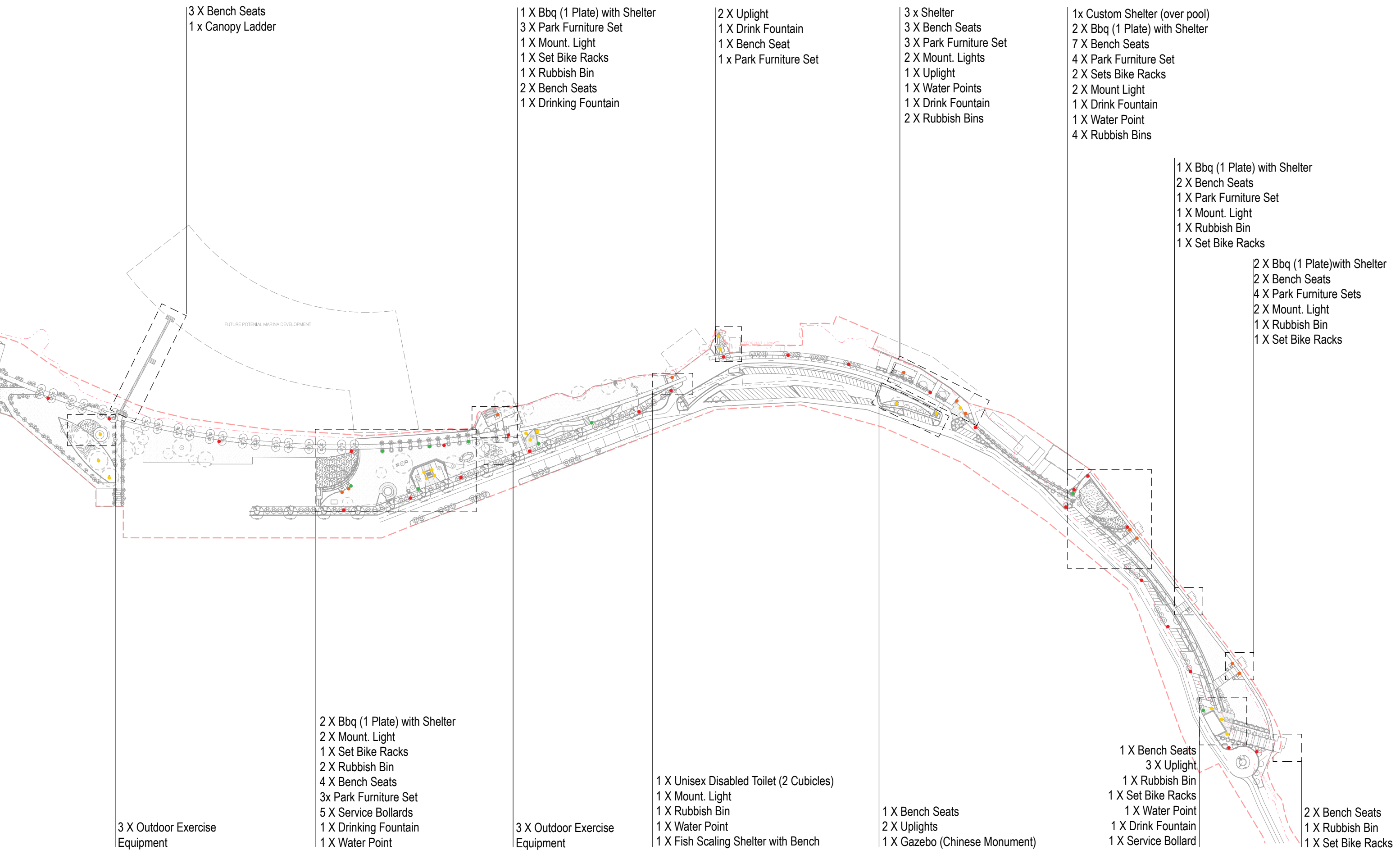
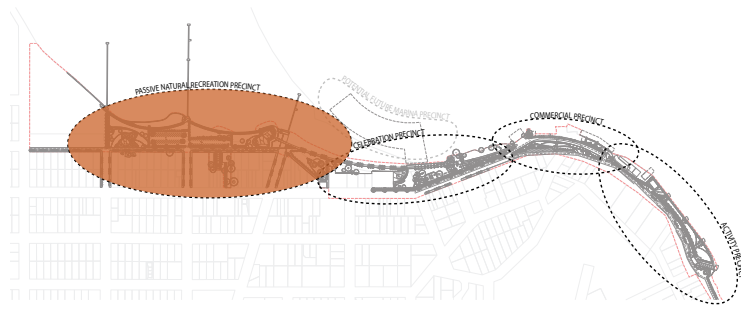
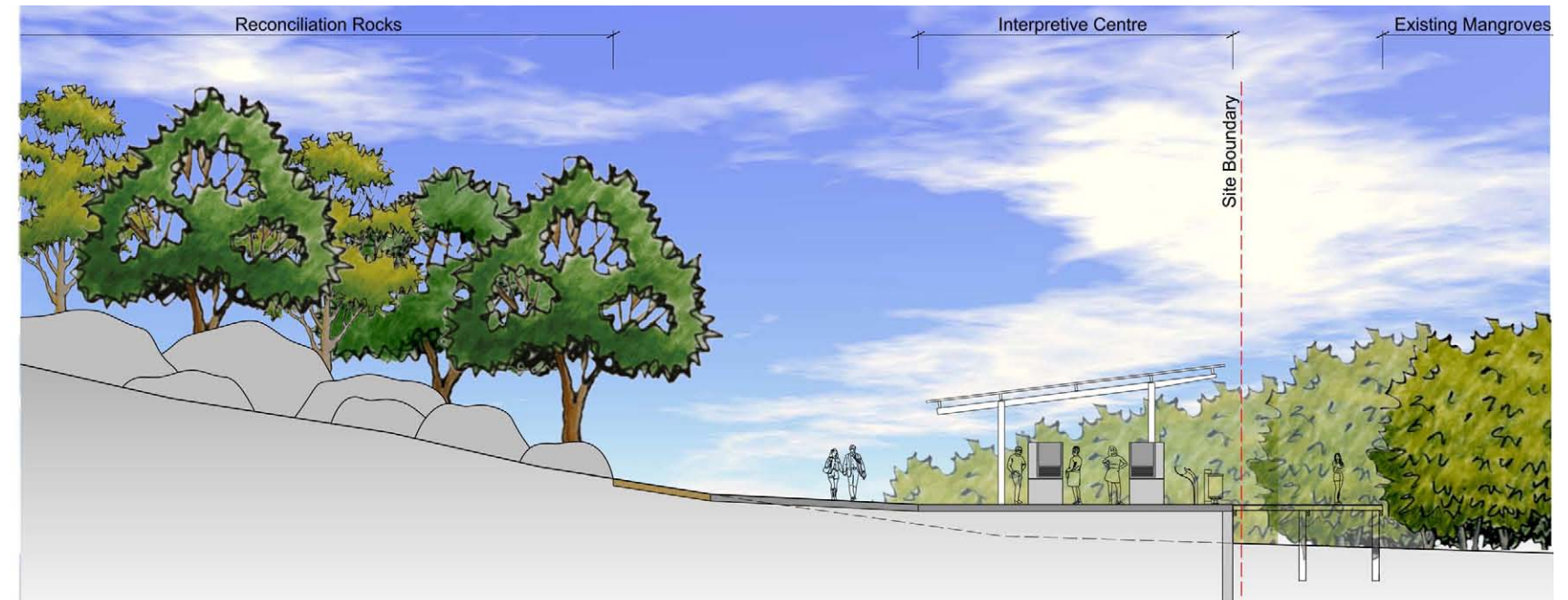


Figure – Cooktown Foreshore & Webber Esplanade Master Plan - Furniture



5.3 The Master Plan Precincts

5.3.1 Passive Natural Recreation Precinct



Section A-A: Scale 1:200 @ A3 - Reconciliation Rocks through Interpretive Centre to mangrove boardwalk



Section B-B: Scale 1:250 @ A3 - Adelaide Street through RV parking to existing mangroves

1. Streetscape treatment: Footpath, shady trees with native groundcovers to base.
2. Main southern pedestrian/cycle connection to foreshore.
3. Connection to existing low tide walk
4. Rail Shunting Yard Interpretive Centre: Carriage remnants integrated into design. Basic directional information to be included to inform RV tourists.
5. Revitalisation of existing rock form: Decomposed granite perimeter with native groundcovers and uplighting.
6. Adelaide Street upgrade: Two way carriageway with streetscaping.
7. Foreshore link: Main shaded pedestrian and cycle link between nodes.
8. Self-contained recreational vehicle short term parking: Water point, dump point

- (sewerage), electricity, rubbish bins, sheltered barbeque and picnic tables.
9. Preservation of heritage granite kerb.
10. Anzac Park Upgrade: Formalised landscape with easy equitable access, increased natural shade, paths, hardstand and lighting. Repositioning and elevation of monuments and cenotaph to better use area and amplify significance.
11. Reconciliation Rocks: Revitalisation to include decomposed granite perimeter, feature up lighting, stamped feature pavement, interpretive centre and feature native groundcover. Potential for monument or statue to recognise Indigenous heritage.
12. Boardwalk: Positioned over existing sewer line to minimise impact on mangroves, viewing platform to look onto adjacent North Shore.

13. Shared path: Restricted vehicle access managed by removable bollards. Path of appropriate vehicle grade and width.
14. Revitalised Open Space: Grassed area with mounded native groundcovers beneath existing mature trees. Mulch rings around some existing trees for preservation of root system. Restricted vehicle access to neighbouring properties and increased natural shade. Strong pedestrian/cycle connection to Charlotte Street/Town Centre.
15. Outdoor Exercise Equipment: Three pieces of equipment per station on a softfall base.



Figure – Scale 1 :2000@A3 | The Master Plan, Passive Natural Recreation Precinct Plan



Figures – Top: Pedestrian/cycle/vehicle path - shared along Adelaide Street through Lion's and Endeavour Parks. Bottom: Short-term RV parking off Adelaide Street with existing mangroves as the back drop.



Figure - Detail Plan - 1:1000 @ A3 - Reconciliation Rocks Interpretive Centre

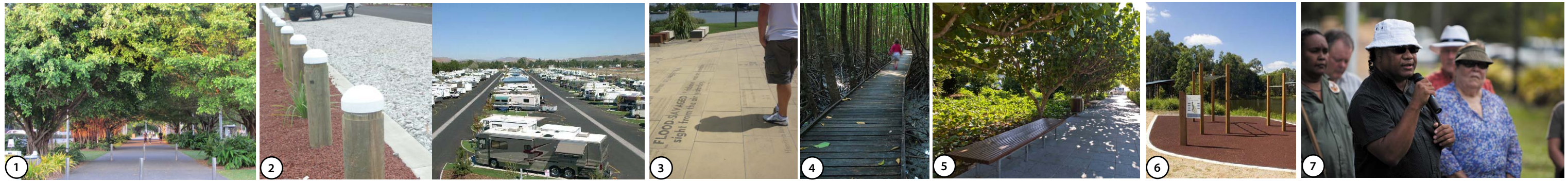
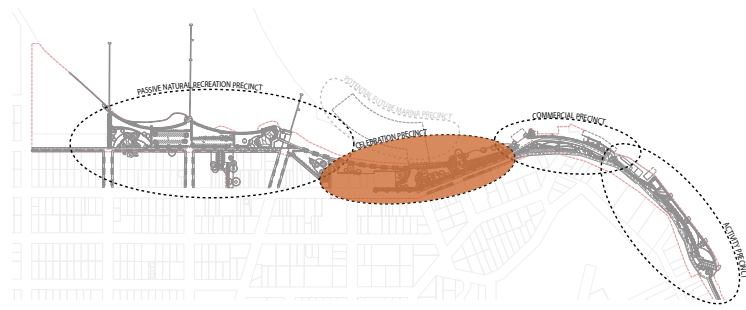


Figure – Indicative Project Images: 1. Shaded pedestrian / cycle path; 2. RV car park using locally sourced materials; 3. Feature stamped pavement that creates another interpretive layer to communicate Cooktown's history; 4. Mangrove boardwalk that is narrow and lightly touches on the landscape; 5. Naturally shaded bench seating; 6. Outdoor fitness stations with softfall base. 7. Opportunity for additional Indigenous based tours.



Figure – Scale 1 :2000 @ A3 | The Master Plan, Hardscape, Passive Natural Recreation Precinct Plan



5.3.2 Celebration Precinct



Section C-C: Scale 1:200 @ A3 - Proposed Openspace through shared path to existing mangroves



Figure D-D: Section - Scale 1:200 @ A3 - Charlotte Street though Endeavour Park

1. Shared path: Restricted vehicle access managed by removable bollards. Path of appropriate vehicle grade and width.
2. Rehabilitation Groundcover: Reinstate and rehabilitate vegetation species that originated in this area.
3. Retain grass areas. Existing trees with mulch rings beneath.
4. Upgrade existing playground: Possibility for installation of softfall similar colours to the Musical Ship.
5. Lion's Park facilities upgrade: Retain existing toilets, Lion's bbq shelter and gazebo. Additional bbq shelters, bike racks, rubbish bins, service bollards and bench seats.
6. Shared Path: Retain existing palms and duplicate pattern of palms on other side of path. Groundcover to the base of palms. Colour concrete banding to give sense of arrival.

7. Access to water's edge: Rock boulder retaining wall with steps leading to narrow existing beach area.
8. Endeavour's Edge Picnic Area: Timber decking over water with barbeque shelters and outdoor furniture. Potential for future relocation of The Australian Volunteer Coast Guard Assoc. Inc. Lease to the Cooktown Blue Water Club Inc. Lease.
9. Retain existing mangrove community to frame Bicentennial Park performance area.
10. Revitalisation of town wells. Upgrade equitable access to the well with stamped feature pavement and feature groundcover around.
11. Revitalisation of Captain Cook Memorial: Increase hardstand around monuments from sidewalk. Elevate smaller monuments to provide more presence. Provide up lighting to all. Terraced embankment with grassed run and concrete riser. Restoration of sandstone elements of the Captain Cook Monument. Small linear

area of turf to soften finish.

12. Outdoor exercise equipment: Three different pieces per station with softfall base.
13. Restricted vehicle exit point (only): Removable bollard to manage movements.
14. Captain Cook Interpretive Centre: Custom Endeavour form that protrudes from the sloped landscape.
15. Grassed terraced amphitheatre: Concrete riser and grassed run. Area to provide more organised seating for events and outdoor movies.
16. Foreshore link with community art pavers inserted.
17. Streetscaping with shaded planted refuges, wide equitable paths, lighting and parallel parking.
18. Boat ramp facilities that include; scaling station, water point, wash down bay, unisex toilet and rubbish bin.



Figure – Scale 1:2000 @ A3 | The Master Plan, Celebration Precinct Plan

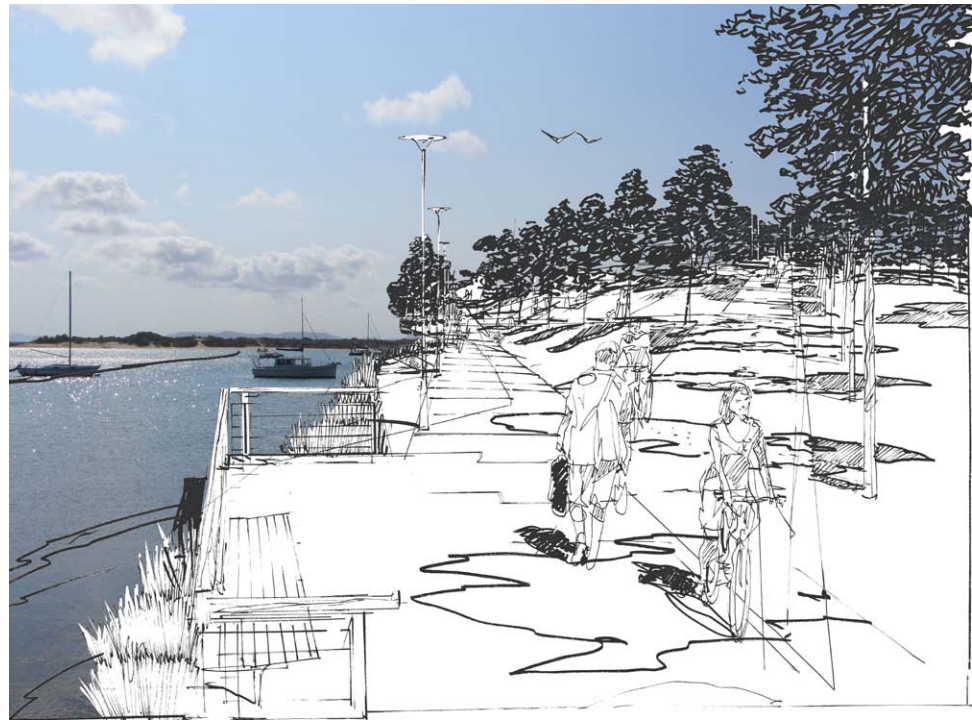


Figure - Top: Foreshore link along foreshore with strong connection to the water's edge.
Bottom: Typical streetscaping, road carriageway and parallel parking.



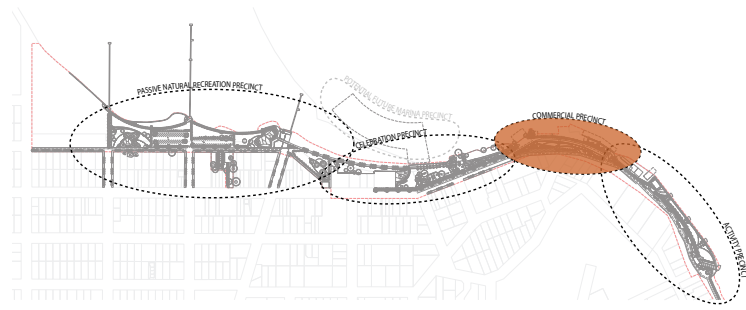
Figure - Detail Plan - 1:1000 @ A3 -Lion's Park & Endeavour Park Upgrade.



Figure – Indicative Imagery & Design Visions - Terrace turf seating to form outdoor amphitheatre for festivals, performances & outdoor movies. Suggested to utilise concrete pylons as steps; Access/egress steps through amphitheatre; Sampled history; and Interpretive Centre reenacting signage.



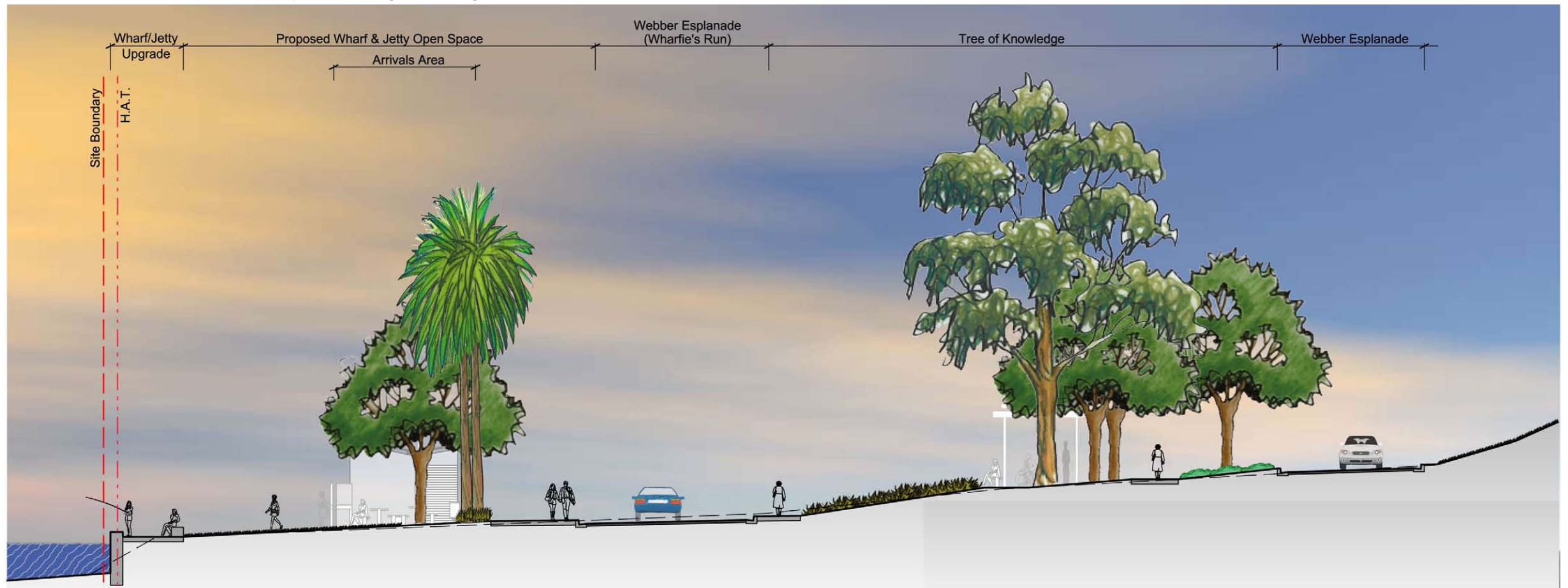
Figure – Scale 1:2000 @ A3 | The Master Plan, Hardscape, Celebration Precinct Plan



5.3.3 Commercial Precinct



Section E-E: Scale 1:250 @ A3 - Webber Esplanade through to existing commercial



Section F-F: Scale 1:200 @ A3 - Webber Esplanade through to Wharf & Jetty

1. Boat and trailer parking: Provisions for 27 drive in/out bays on the suspended slab
2. Streetscaping – typical design layout. Provisions will be made for private property entrances.
3. Boat Ramp Upgrade - Under CSC Management
4. Queen's Steps and Mick the Miner Revitalisation: Gentle and respectful restoration of area around significant monuments with increased hardstand, up lighting and picnic facilities.
5. Existing Commercial Lease: With land and water access.
6. Foreshore Link: Shaded pedestrian/cycle path along perimeter of existing commercial establishments.
7. Waste Disposal Area: Relocation of waste disposal area with upgrade to monitored skip system. Provisions for aesthetic screening and vehicle access to bins. (Orange outlined indicative area)
8. Parallel Parking for Commercial Area: 34 short term time allowance bays
9. Commercial Opportunity - beside existing leaseholders.
10. Jetty/Wharf Upgrade: Provisions for commercial wharf to be separated from recreational public jetties. Additional moorings for boats that need to dock and restock. Separate design investigation required.
11. Jetty Openspace: Additional natural shade, sheltered picnic tables, water points, rubbish bins and turfed area. Provisions retained for 14 tonne truck access to operational wharf.
12. Wharfie's Run: Road realignment that will retain existing viewsheds, provide pull over bay and loop circuit.
13. Public Toilet and Waterfront Arrivals Kiosk: to replace existing toilet block. Cruise/ Tender Boat visitor arrivals kiosk with distinct 'Cooktown' character. Sheltered facilities provided include: toilets, showers, lockers, information signage, bike hire and racks, rubbish bins, drinking fountain, service bollard, water point and kiosk (sale of smaller items). Feature stamped pavement with pedestrian / cycle shaded foreshore links back to town centre. Restoration and elevation of existing propeller remnant.
14. Cooktown Blue Water Club Inc. Parking: 13 bays for disabled and general parking.
15. Existing Cooktown Blue Water Club Inc. Leasehold Land: Relocation of the Australian Volunteer Coast Guard Assoc. Inc. under investigation. Leasehold area excluded from scope of master plan.
16. Possible Australian Volunteer Coast Guard Assoc. Inc Boat Pen: Ample area around for access.
17. Emergency Vehicle Parking.
18. Foreshore Link: Pedestrian / cycle path lined with palm trees.
19. Proposed Commercial Opportunities: Row of commercial establishments built beneath suspended slab opposite to existing establishments. Embankment recessed back, closer to existing road corridor of Webber Esplanade to allow for a more effective use of area that incorporates economic opportunities, boat trailer parking, angled parking, safe pedestrian movements and retention of viewsheds.
20. Long Term Vehicle Parking: (45 bays) For example, staff parking.
21. Tree of Knowledge & Chinese Monument Openspace: Retain existing bench seat and artefacts under the Tree of Knowledge. Relocate Chinese Monument, reinstate coloured concrete plinth base with Chinese style gazebo overhead.



Figure – Scale 1 :2000 @ A3 | The Master Plan, Commercial Precinct Plan



Figure - Indicative Project Imagery - 1. Retain the majority of white post and rail fencing in this area. Option to reinstate fence along upgraded streetscape later. 2. Unisex toilets near boat ramp. 1 x disabled cubicle and 1 x regular cubicle. Gable roof version of this design has been costed. 3. Fishing Cleaning Shelter with bench and water point. 4. Retain views to water with Commercial Precinct.

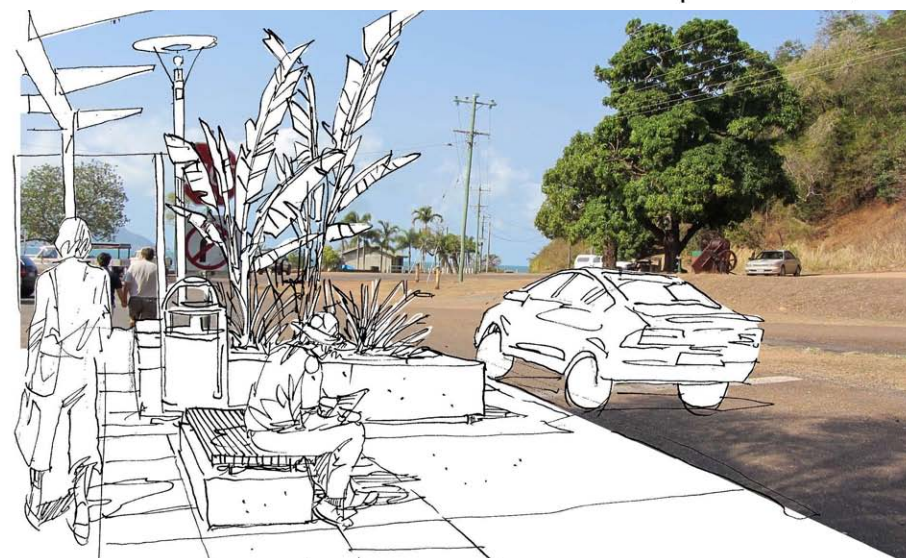


Figure -Top: Increased commercial opportunities for primarily retail and hospitality establishments. Bottom: One way road movements with clear separation between pedestrians and vehicles. Upgraded footpaths and parallel parking.

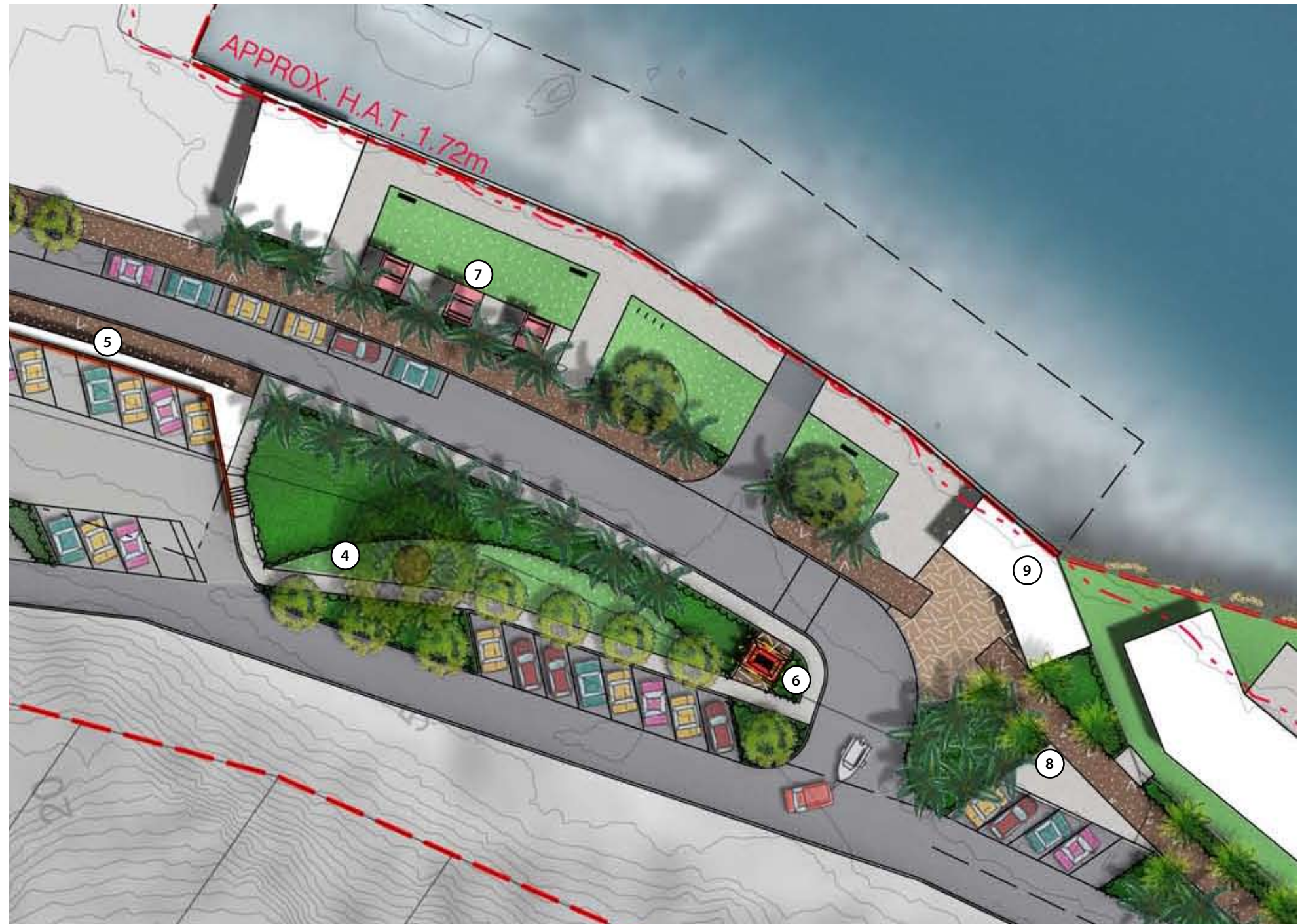


Figure - Detail Plan - 1:1000 @ A3 - Traffic Management and Tree of Knowledge / Chinese Monument Openspace.



Figure – Indicative Project Imagery - 5. Commercial Opportunities along lower level of Webber Esplanade, beneath suspended slab. Design to encourage outdoor dining. 6. Chinese style gazebo over relocated Chinese Monument. 7. Sheltered picnic tables. This design has been recommended for costing purposes. This gable shelter design is able to be lowered during severe weather conditions, protecting barbeques, furniture and the structure. 8. Mounted propeller to be positioned beside Public Toilets / Arrivals Kiosk. 9. Public Toilets / Arrivals Kiosk - small built structure with an ecological and sustainable design. 10. Boat pen provisions suitable for Coast Guard vessel type.

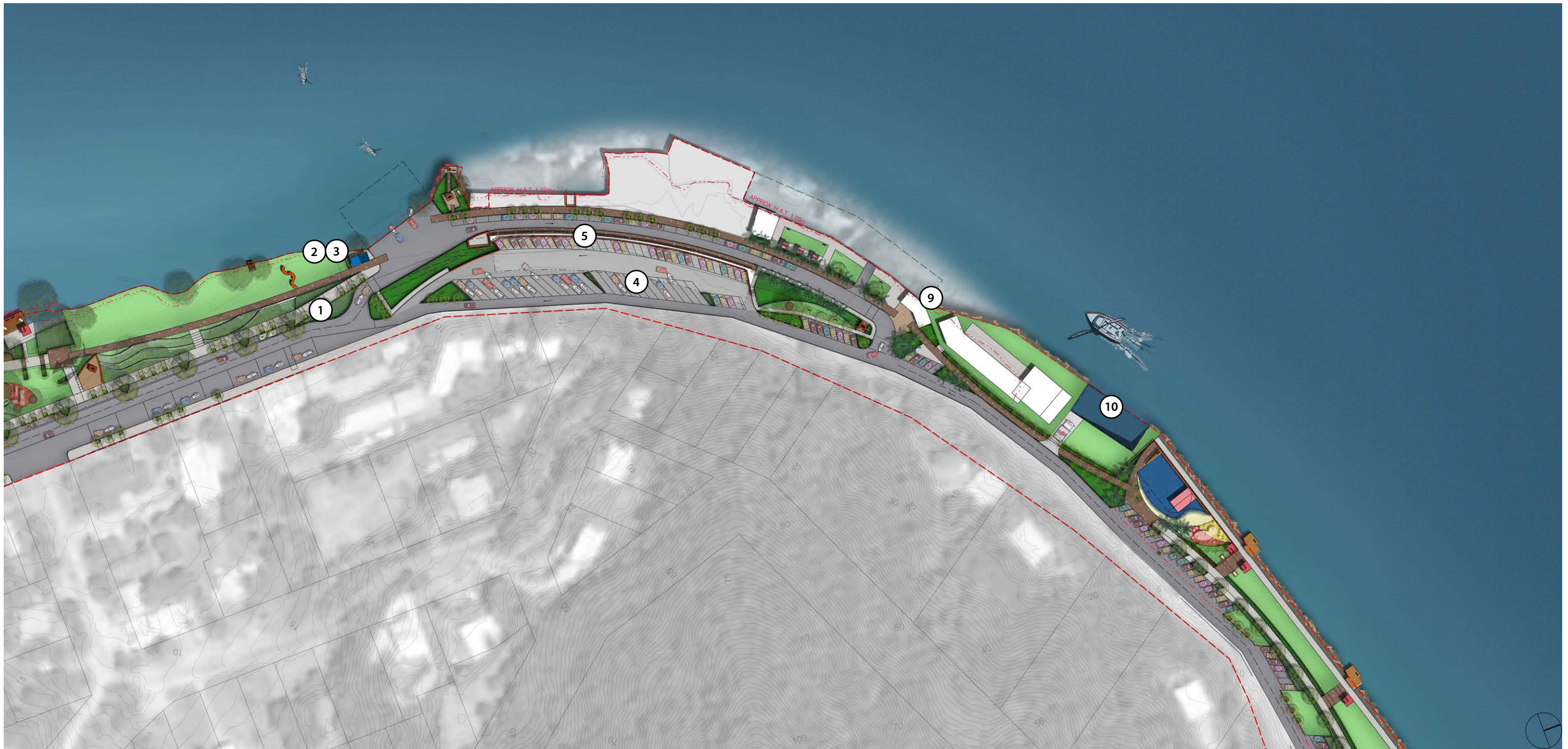
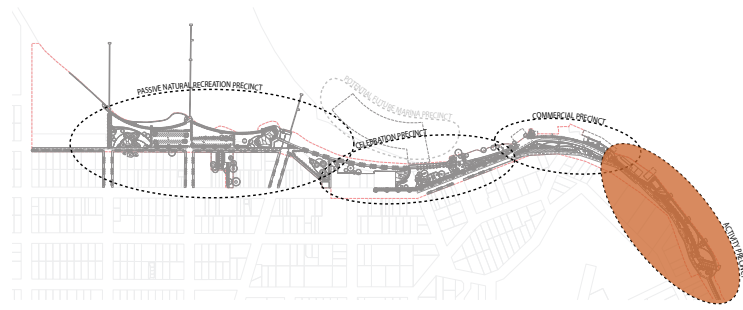
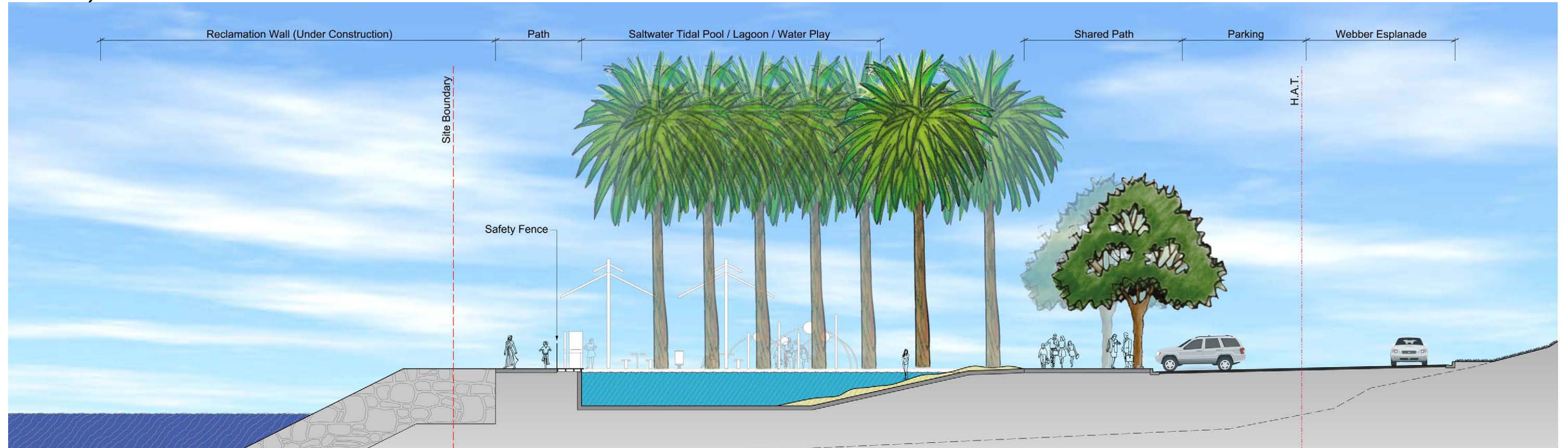


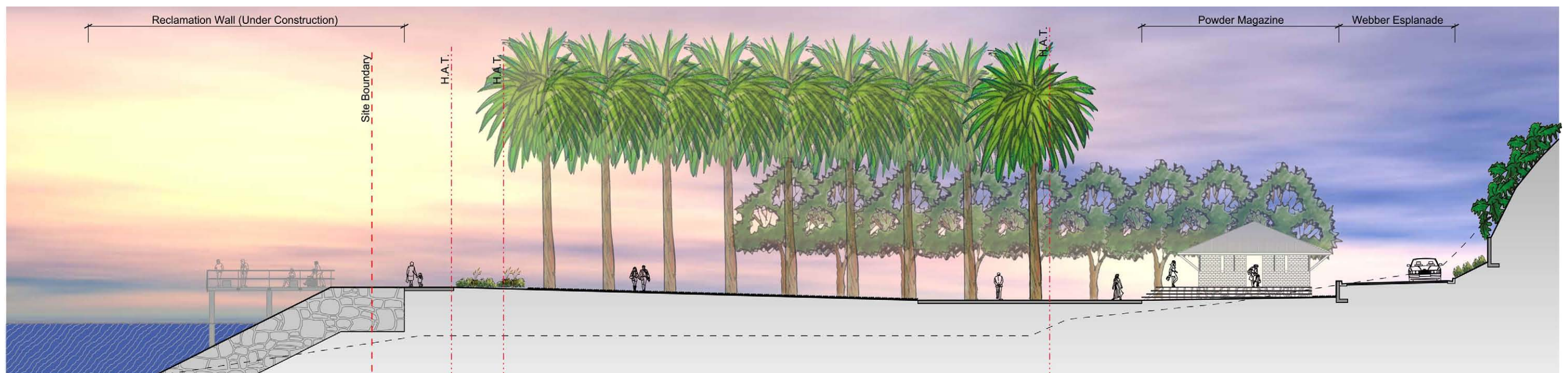
Figure – Scale 1 :2000 @ A3 | The Master Plan, Hardscape, Commercial Precinct Plan



5.3.4 Activity Precinct



Section G-G: Scale 1:250 @ A3 - Webber Esplanade through saltwater tidal lagoon to reclamation wall



Section H-H: Scale 1:250 @ A3 - Webber Esplanade through Powder Magazine Openspace to Fishing Pontoons

1. Foreshore Link: Pedestrian/cycle path lined with palm trees.
2. Lagoon Parking (21 bays)
3. Saltwater Tidal Lagoon: Beach themed secure swimming facility. Fenced perimeter. Self-flushing system with sandy bottom, sloped entry for all abilities. Partially sheltered (shown as pink rectangle) for protection from environmental elements.
4. Water Play Park: All abilities, fenced playground with softfall base. Water Play equipment to use reticulated saltwater. Naturally shaded bench seats, bike racks, rubbish bins, service bollard, water point and beach shower.
5. Pontoon Sheltered BBQs and Picnic Facilities: Aligned with the cantilever fishing pontoon to encourage family outings. Well lit area with rubbish bins, bike racks and strong connection to Foreshore Links.
6. Fishing Pontoon: Cantilever structure protruding over water to enable deep water fishing. Bench seats and strong connection along foreshore provided.
7. Additional Parking (21 bays)
8. Webber Esplanade: Resurfaced, minor re-alignment, two way carriageway with private property access provisions, underground power service and street lighting.
9. Foreshore Link: Shaded pedestrian / cycle path to connect foreshore to Powder Magazine. Shady trees on eastern side of path to ensure viewsheds are not blocked.
10. Reclamation Wall: Under Construction.
11. Powder Magazine Revitalisation: Timber deck and steps around perimeter of building to enable viewing into the structure as well as create elevated views to water. Window panels replaced with reinforced glass / perspex. Feature stamped concrete around building to create another interpretive informative display. Increased hardstand and palms lead to and frame the final foreshore pontoon and its view.
12. Fishing Pontoon: Largest of the four cantilever pontoons.
13. Vehicle roundabout: Designed to enable buses and vehicles the ability to manoeuvre and return direction towards town.
14. Private estate and properties beyond.



Section I-I: Scale 1:250 @ A3 - Webber Esplanade through Powder Magazine Openspace to Fishing Pontoons



Figure - Scale 1:2000 @ A3 | The Master Plan, Activity Precinct Plan

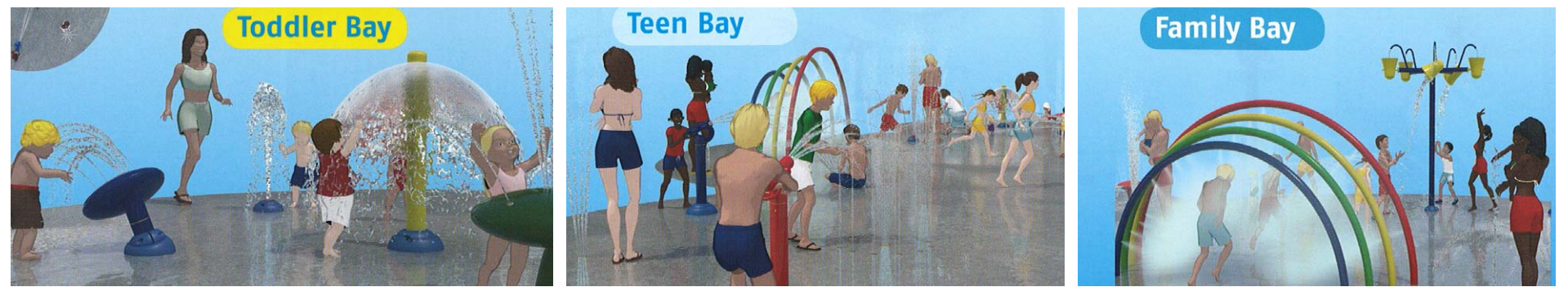


Figure – Indicative Project Imagery: Waterplay Park Equipment, opportunity to design three zones suitable for all abilities and age groups.



Figure – Top: Typical Reclamation Wall Section - Through path and lagoon. Indicative signage shown. Bottom: Powder Magazine Openspace



Figure - Detail Plan - 1:1000 @ A3 - Saltwater Tidal Lagoon



Figure – 1. Saltwater Tidal Lagoon with size provisions for fitness and training; 2. Waterplay park with softfall base. All abilities equipment; 3. Tropical beach theme. 4. Pontoons for fishing and viewing adjacent landscape; 5. Shaded foreshore link with clear separation from road and open views to water; 6. Plaza treatment that frames and leads to water's edge.

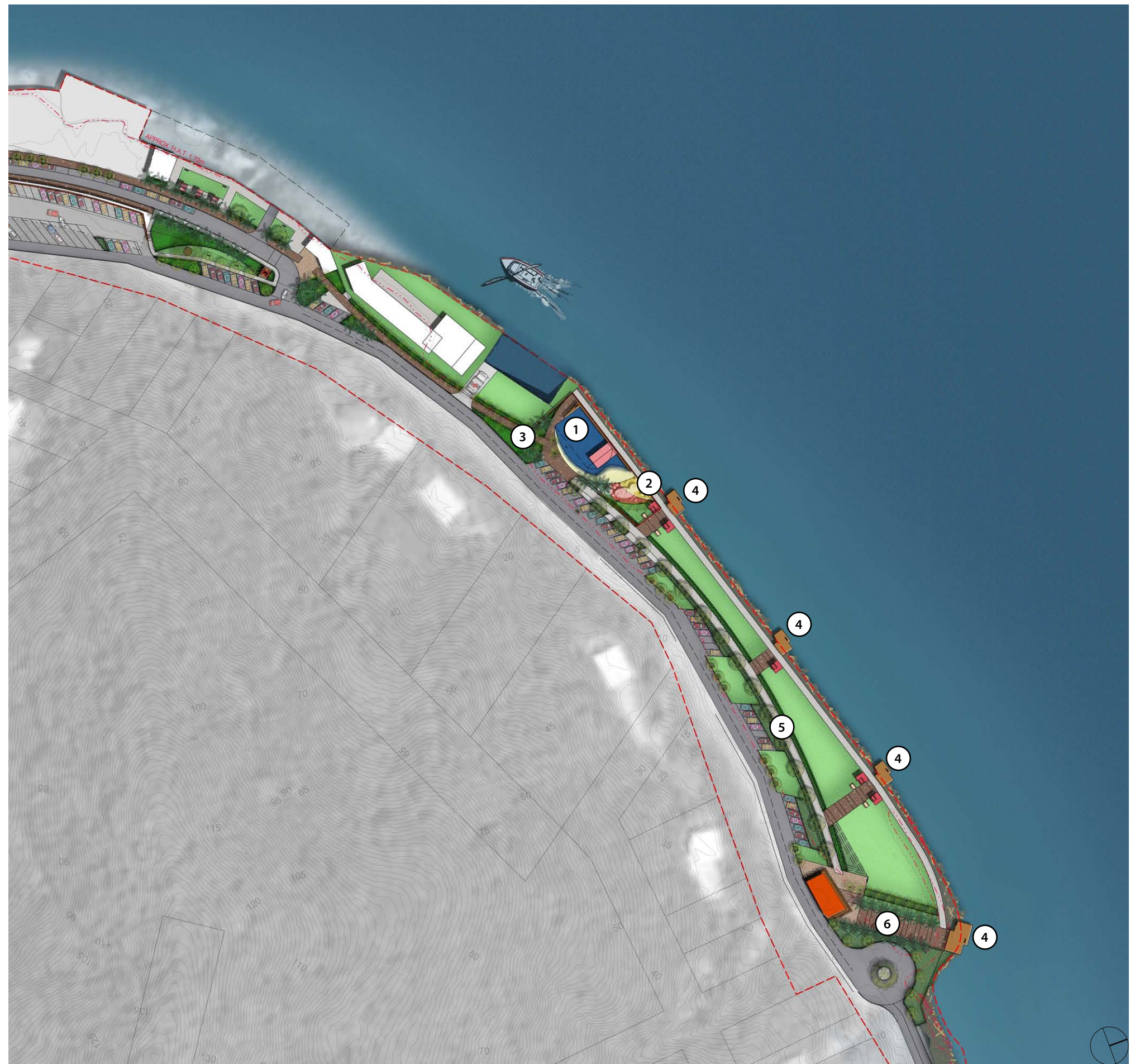


Figure – Scale 1 :2000 @ A3 | The Master Plan, Hardscape, Activity Precinct Plan

5.4 Land Uses

5.4.1 Existing Land Use Plan

The following analysis of existing land uses has been based upon the Cooktown Locality Plan (2007), that illustrates the current zones of land uses and community uses -

Brief analysis of each existing land use in comparison to the actual use it currently performs:

- Business: This land use is present in two locations. The first is positioned alongside Adelaide Street from Hogg Street to north side of Furneaux Street, separated only by Anzac Park. The second is at the northern end of Charlotte Street. Mixed businesses occupy the majority of these lots and are orientated onto Charlotte Street. The proposed Master Plan recommends the inclusion of an additional commercial land use between Furneaux Street and the existing nearby community use.
- Conservation: The main conversation areas that share the site boundary with this Master Plan are the large mangrove community, that wraps around the Endeavour River, and the Grassy Hill bushland. These areas have been relatively preserved with the inclusion of recently constructed recreational provisions to support local tourism.
- Community Use: The areas that have been identified as Community Use are a combination of commercial and community organisation leases. Please refer to the Stakeholder sections for more information about lease holders.
- Industrial: There are no industrial land uses positioned within or adjacent to the site boundary.
- Medium Density Residential: The land use is located along Charlotte Street between Banks and Hall Streets, and the majority of these lots are occupied or have developments under construction.
- Mixed Use: This area, positioned alongside Adelaide Street, is currently vacant. This area is out of the scope of works and has remained zoned as Mixed Use.
- Openspace: The majority of the foreshore is designated as open space. This existing zoning has been maintained with minimal or nil conflict of land uses.
- Low Density Residential: This area, positioned on the southern side of Webber Esplanade, is currently occupied by dwellings, with some having

road (driveway) access to the esplanade.

- Rural Residential: There are no rural residential land uses positioned within or adjacent to the site boundary.
- Rural: There are no rural land uses positioned within or adjacent to the site boundary.

5.4.2 Proposed Land Use Plan

The proposed foreshore Master Plan looks to make minimal impact on the changes to land uses. The recommended uses revisions can be viewed in the Proposed Land Use Plan (right). The revisions are:

- Extension of 'open space' along the majority of the foreshore;
- Proposed relocation of 'Community' use Coast Guard lease to merge with proposed 'Business' use (also currently known as Cooktown Blue Water Club Inc. Lease);
- Existing wharf area 'Community Uses' (commercial establishments) extended as proposed 'Business';
- Road reserve connecting to Charlotte Street, north of Furneaux Street, proposed to be closed and rezoned as 'Business';
- Road reserve known as Adelaide Street to be closed and rezoned as open space north from Furneaux Street onwards; and
- 'Conservation' zone that includes mangrove community to be marginally rezoned as 'open space' with linear areas spanning into the vegetation from the existing open space.
- 'Potential Future Marina Precinct' - although this precinct has not been costed into the scope or shown on the Land Use Plan, provisions have been made for the future proposed marina.

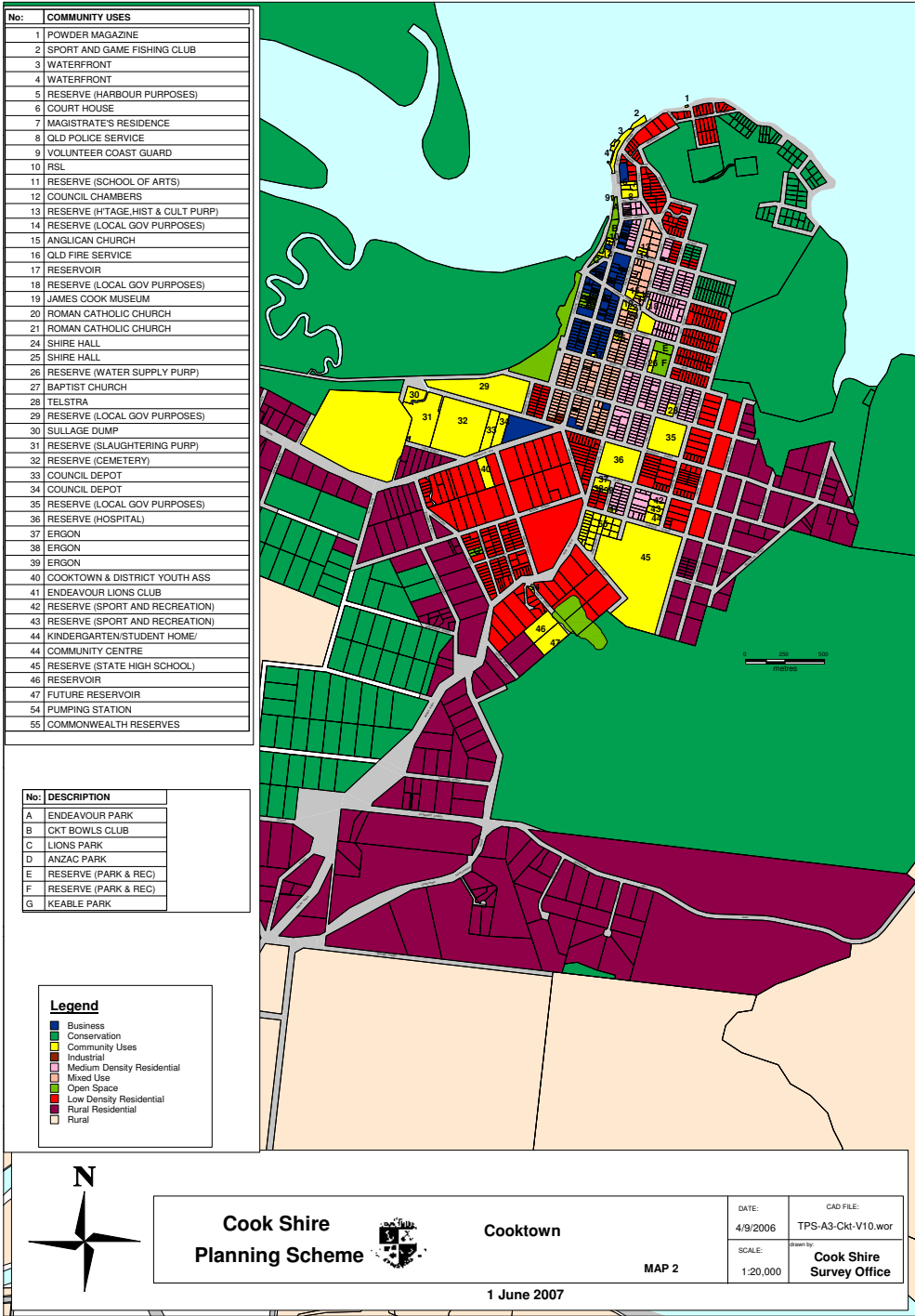


Figure – CSC Cooktown Locality Plan

Cooktown Foreshore & Webber Esplanade Revitalisation



Figure – Proposed Land Use Plan

5.5 Prioritised Future Needs Assessment & Corresponding Infrastructure Demand

5.5.1 Analysis of Existing Infrastructure

The Cooktown foreshore area is serviced by the following existing infrastructure systems:

- Roads and Drainage;
- Water Reticulation;
- Sewerage Reticulation; and
- Electrical and Telecommunications.

See below for further details of the existing systems.

5.5.2 Roads and Drainage

The majority of the streets within the project vicinity are sealed, with the exception of Adelaide Street. There is no consistency with respect to carriageway seal width for streets that are of a similar hierarchy. Cook Shire Council (Council) could not provide any as constructed records or mapping of existing stormwater systems and culverts.

Details of each road are discussed below-

- Hogg Street: The Hogg Street carriageway appears to be approximately 10m wide and is bound by granite kerb and channel on both sides of the road. The kerb and channel has been removed to accommodate a vehicle crossover to the bakery (northern side of reserve). There is a distinct lack of constructed drainage and stormwater runoff flows along the kerb invert and beneath Adelaide Street via a culvert crossing.
- Walker Street: The width of seal provided to Walker Street appears to be substantial (greater than 10m wide) and there is sufficient space for angle parking on the northern side of the road reserve. The road is bound on both sides by granite kerb and channel and a concrete ramp is provided for a vehicle access crossover to the Cooktown Orchid Travellers Park. Once again, there is no constructed drainage and stormwater runoff is accommodated in the kerb until it is discharged beneath the Adelaide Street intersection.
- Furneaux Street: The seal width provided to Furneaux Street is quite narrow, however the road reserve width is approximately 30m wide and is of sufficient width to accommodate any road width upgrades. The roadway is bound by granite kerb and channel on the northern side of the road (immediately adjacent to the edge of seal), while there is no kerb in place on the southern side. The constructed kerb invert deepens towards the western end of the street to accommodate the discharge from an underground drainage culvert.
- Adelaide Street: Adelaide Street is unsealed and is currently a gravel track. Heritage granite kerbing is provided to the western side of the track. There appears to be some established vegetation within the road reserve and this will need to be cleared to facilitate an upgrade to the carriageway.
- Charlotte Street: Charlotte Street is the main street through Cooktown and is bounded predominately on both sides by commercial premises. The Charlotte Street seal width is considerable and can accommodate

angle parking on both sides of the street. The road is bound by granite kerb on both sides and it has been removed in places to provide vehicle access to adjacent properties. There appears to be a lack of constructed drainage, with culverts provided to convey water beneath intersections. The road crossfall is considerable and this will limit roadway flow widths prior to the kerb over topping.

- Webber Esplanade: Webber Esplanade is the continuation of Charlotte Street. Roadway seal widths are substantially narrower than what is provided to Charlotte Street (i.e. in the vicinity of a 4m – 5m seal width). Webber Esplanade provides access to a future development area and therefore road usage will be limited. Land reclamation works are currently underway on the western side of the road reserve. The road reserve is encumbered by an existing overhead electricity line located on the eastern side of the road reserve.

5.5.3 Water Reticulation

Existing water reticulation information was sourced from Council and indicates that there is a water main located on the eastern side of Charlotte Street. In addition, the mapping indicates that connections have been provided to the commercial precinct and along Furneaux Street. Council indicates that the water reticulation main is 150mm diameter south of Bank Street and 100mm diameter to the north of Bank Street towards Webber Esplanade. Council reports there are no pressure issues associated with the infrastructure.

5.5.4 Sewerage Reticulation

Existing sewerage as constructed information was provided by Council, however it is noted that they do not include the original Cooktown sewerage scheme that was constructed in the 1980s. Council does not have any electronic information relating to this. Furthermore, Council also advised that the as constructed information relating to the northern end of Charlotte Street and Webber Esplanade could not be found.

From the information provided, it would appear that a gravity sewerage reticulation system is provided within the project area. Discussions held with Council staff indicate the project area is serviced by three pump stations. Pump Station 2 services the area along Charlotte Street to the north of Green Street, Pump Station 3 services an area between Green Street and midway between Hogg and Walker Streets while Pump Station 4 services the areas to the south of Hogg Street. Council indicates that there are no capacity issues in the gravity lines or at the pump stations.

5.5.6 Electrical and Telecommunications

Dial before you dig information was sourced and this indicates that above ground electricity and telecommunications infrastructure is provided to Charlotte Street. No indications were received relating to any capacity issues associated with this infrastructure.

5.6 Future Infrastructure Outcomes - ‘The Shopping List’

5.6.1 Future Infrastructure Outcomes – ‘The Shopping List’

Proposed infrastructure sketch plans have been prepared to identify how the proposed foreshore developments will be serviced. Upgrades will be required to the following infrastructure systems:

- Roads and drainage;
- Water reticulation;
- Sewerage reticulation;
- Off-street parking areas;
- Electricity; and
- Tidal Pool

Further details appear below.

5.6.2 Roads and Drainage

Based on the high level investigations undertaken as part of this master planning process, typical sections of Hogg, Walker, Furneaux and Charlotte Streets and Webber Esplanade have been developed. These sections indicate the minimum roadway width that will be required, however there is insufficient information available to ascertain the scale of upgrade.

Further commentary on these sections is provided below-

- Hogg, Walker and Furneaux Streets: Given that it is proposed to construct a new RV parking area located off Adelaide Street, these streets will need to be of sufficient width to accommodate the associated vehicle manoeuvres. A minimum carriageway width of 10m is proposed and this will accommodate two-way traffic flow (not inclusive of on-street parking requirements). Considerable upgrade works will be required on Furneaux Street due to the limited width of bitumen seal currently provided. In addition, upgrades will need to be sensitive to the existing granite kerbing which is heritage listed and cannot be modified without entering into a substantial approvals process. As a result, it will not be possible to construct a comprehensive underground drainage system for these roads.
- Adelaide Street: It is proposed to provide a formal, sealed carriageway (7.5m minimum width) to Adelaide Street to accommodate two-way operation of RVs. Upgrades (particularly drainage and access crossovers) will need to be sensitive to the existing heritage listed granite kerbing located to the western side of the road reserve. No on-street parking will be provided.
- Charlotte Street: It is proposed to provide formal arrangements in the commercial area, to effectively manage the anticipated vehicle and pedestrian manoeuvres. Dedicated parking areas are proposed for vehicles and boat trailers while a pedestrian management strategy will need to be developed to ensure crossing locations and potential pedestrian and vehicle conflict points are considered. In addition to this, a system of one-way roads is proposed and this ensures that the parking is more safely accessed. A speed limit of 40km/hr is recommended throughout this area. Pavement area will be provided such that cars and boat trailers can drive in and out of the parks without requiring a reversing manoeuvre. Given the level difference between the commercial precinct and Charlotte Street, there is insufficient space to accommodate a landscape batter. Therefore a retaining wall

will be required. Ultimately, Council has indicated that this area will be used for additional commercial buildings. At this time, some effort will need to be made to co-ordinate the parking with the footprint of the buildings. One possible solution may be to use the roof of the proposed building to provide additional parking space for cars and boat trailers (27 bays) and staff vehicles (45 bays).

Provision for a bus stop has been provided on Charlotte Street on the Cooktown side of the main commercial precinct. This will enable buses to set down passengers and then use the commercial precinct turn around area to return to Cooktown.

Parallel parking bays (34) are provided for cars within the commercial area. Bays have been assigned a conservative 6.3m length and provide an indication of the number of parks that can be accommodated. In addition, the parking lane width is very conservative and it may be possible to optimise it once detailed survey is received.

Parallel parking bays (8) for car and boat trailers are provided on Charlotte Street. 15m long parking bays are indicated, and this length will need to be confirmed during the detailed design. Parking bays are located at an offset of 15m from the intersection of Charlotte and Hill Streets. In addition, it will be possible to optimise the width of the parking aisle once details survey is received.

- Webber Esplanade: It is proposed to upgrade Webber Esplanade to a 7.5m carriageway width. The existing land reclamation works that are currently underway will provide sufficient space on the western side of the Esplanade to accommodate the proposed foreshore development.

As part of the Esplanade Upgrade, it is proposed to provide on-street parking. In order to maximise the number of parks available to Council, both perpendicular and angle parking options were considered. While the perpendicular parking arrangement can be accessed be vehicles travelling in either direction, they require greater roadway width and as a result would encroach within the proposed foreshore development area. As a result, the angle parking arrangement was preferred.

A turn around facility will be required at the end of Webber Esplanade to enable vehicles to return to Cooktown.

5.6.3 Water Reticulation

In order to adequately service the proposed foreshore redevelopment, it is proposed to extend the existing water reticulation network. Pipework is nominally identified as DN100 and will connect to the existing system on Charlotte Street. Further network modelling may be required to confirm that adequate pressures can be delivered by the proposed water main.

5.6.4 Sewerage Reticulation

Minor gravity sewerage connections will be required to service the RV septage receiving manhole and the proposed public toilets located within the commercial precinct at the wharf. Further investigations will need to be undertaken, particularly at the commercial precinct to confirm that the public toilet block can connect to the existing system using by gravity. Gravity pipes will be 150mm diameter.

5.6.5 Off Street Parking Areas

Off street parking areas are proposed to be located on Adelaide Street. These parking areas provide 39 RV parking spaces. The works will need to be closely co-ordinated with existing services.

5.6.6 Electrical / Telecommunications

Electrical connections will be required to service the proposed development works. Further consultation will be required with Ergon to confirm that the existing system has adequate capacity and the location where a point of supply can be provided. In addition, it is proposed to remove the existing overhead power line constructed along Webber Esplanade and replace it with an underground system.

5.6.7 Tidal Pool

A tidal pool is proposed as part of Stage 2 of the foreshore development. This will be located to the north of the commercial precinct on Webber Esplanade. The tidal pool concept has been developed to minimise the amount of routine maintenance that will be required. Pool water levels will rise and fall depending on the tidal cycle to enable a regular turnover of stored water (in lieu of chemically treating the water).

Consideration was given to the bottom level of the pool to provide Council with sufficient opportunity for routine maintenance and cleaning. In order to achieve this, tidal data was extracted for the month of February 2009.

From this data, it was possible to calculate average high and low tide levels for February and the average low tide level was used to set the level of the bottom of the pool. This was a very simplistic analysis and further consideration will need to be given to the following prior to finalising the bottom level:

- Minimum and maximum desirable pool depths;
- Required frequency of routine maintenance; and
- Required frequency of complete storage turnover.

5.7 Infrastructure Prioritisation

Based on the proposed master plan staging, the following infrastructure works will be delivered in separate precincts.

5.7.1 Passive Natural Recreation Precinct

The following infrastructure will be delivered as part of the Passive Natural Recreation Precinct:

- Upgrades to Adelaide, Hogg, Walker and Furneaux Streets;
- Provision of off-street parking;
- Extension of the water reticulation system connect to the existing system in two locations on Charlotte Street;
- Connection of the proposed RV septage receiving pit to the existing system;
- Provision of water and electrical connections for the infrastructure network.

5.7.2 Celebration Precinct

The following infrastructure will be delivered as part of Celebration Precinct:

- Water, sewerage and electrical connections especially to proposed lighting, BBQs, service bollards and public toilets; and
- Road upgrades to accommodate new parking arrangements.

5.7.3 Commercial Precinct

The following infrastructure will be delivered as part of Commercial Precinct:

- Water, sewerage and electrical connections especially to proposed lighting, BBQs, service bollards and public toilets; and
- Road upgrades to accommodate new parking arrangements.

5.7.4 Activity Precinct

The following infrastructure will be delivered as part of Activity Precinct:

- Water and electrical connections to taps, BBQs and service bollards as required.
- Road upgrades to accommodate new parking arrangements.



Roads and Drainage
Hogg, Walker, Furneaux, Adelaide and Charlotte Streets, as well as Webber Esplanade.



Water Reticulation
Extend the existing network. Pipework nominally DN100.



Sewerage Reticulation
Minor gravity sewerage connections. Gravity pipes 150mm diameter.



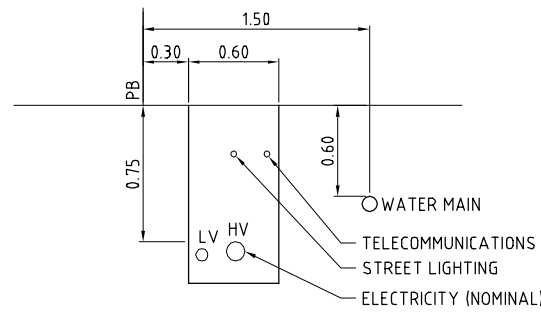
Off-street Parking Areas
Self-contained RV Parking, as well as, vehicle & trailer parking.



Electricity
Replace with underground system. Electrical connections required. Consultation Ergon required.



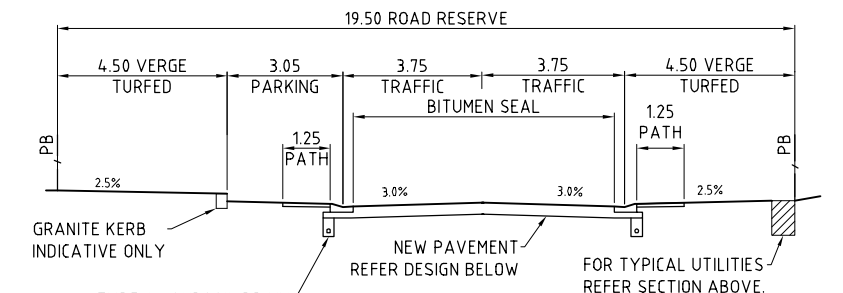
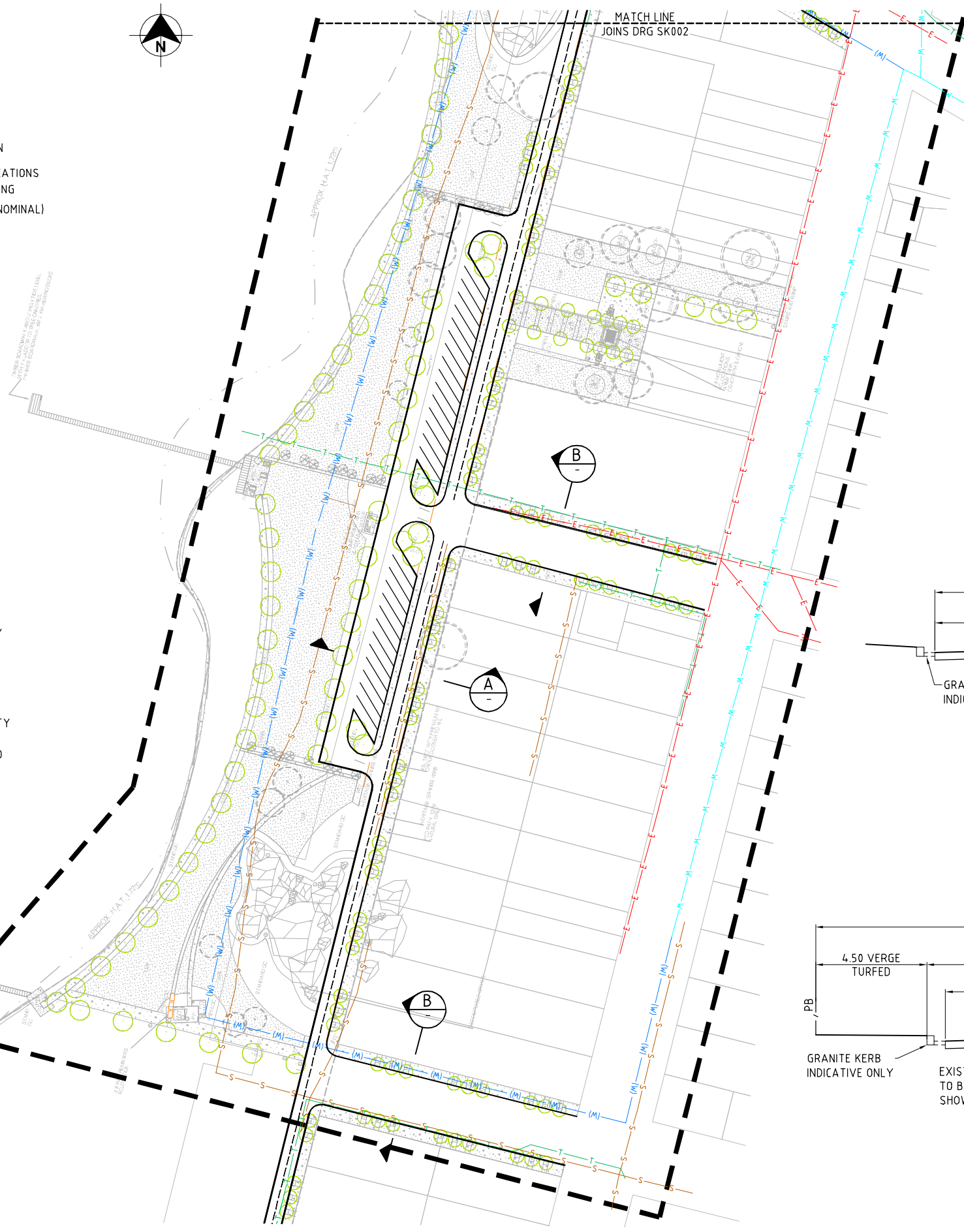
Tidal Pool
Pool level will rise and fall (slightly) with tidal cycle.



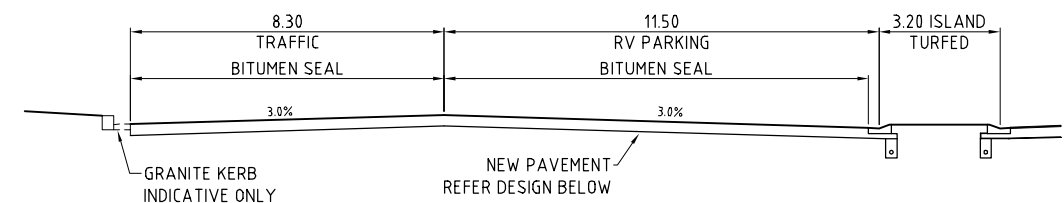
**TYPICAL UTILITIES
TRENCH ARRANGEMENT**
NOT TO SCALE

LEGEND

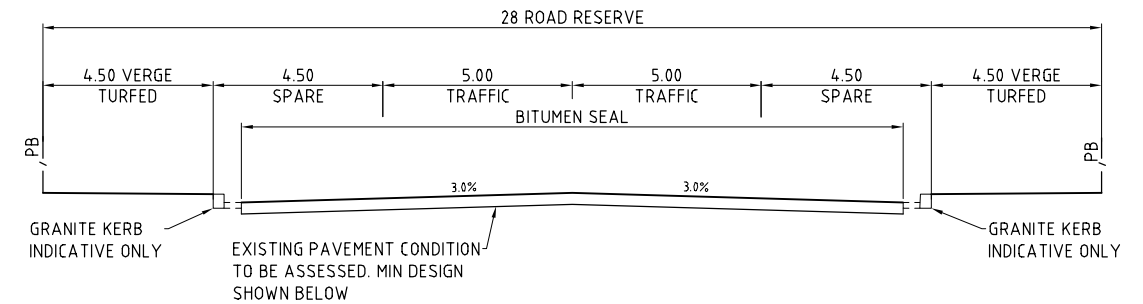
- EXTENT OF WORKS
- - - S - - - EXISTING SEWER MAIN
- - - W - - - EXISTING WATER MAIN
- - - T - - - EXISTING TELSTRA
- - - HV - - - EXISTING OVERHEAD ELECTRICITY
- - - E - - - EXISTING UNDERGROUND ELECTRICITY
- (w) (w) PROPOSED WATER MAIN
- (s) (s) PROPOSED SEWER MAIN
- EUG PROPOSED UNDERGROUND ELECTRICITY
- ***** EXISTING SERVICE TO BE ABANDONED



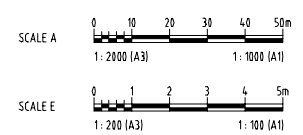
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ADELAIDE STREET**
SCALE E
PAVEMENT DESIGN
2 COAT BITUMEN SPRAY SEAL
7/14mm AGGREGATE
WITH PRIMER SEAL.
150mm BASE CBR 60
150mm SUBBASE CBR 45



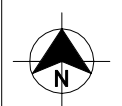
**TYPICAL SECTION A
ADELAIDE STREET**
SCALE E
PAVEMENT DESIGN
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7/14mm AGGREGATE
WITH PRIMER SEAL.
150mm BASE CBR 60
150mm SUBBASE CBR 45



**TYPICAL SECTION B
HOGG STREET
WALKER STREET**
SCALE E
PAVEMENT DESIGN
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WITH PRIMER SEAL.
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150mm SUBBASE CBR 45

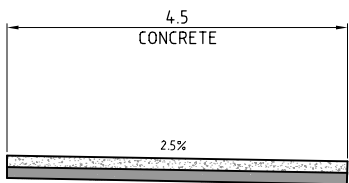


CONCEPT DESIGN
NOT FOR CONSTRUCTION



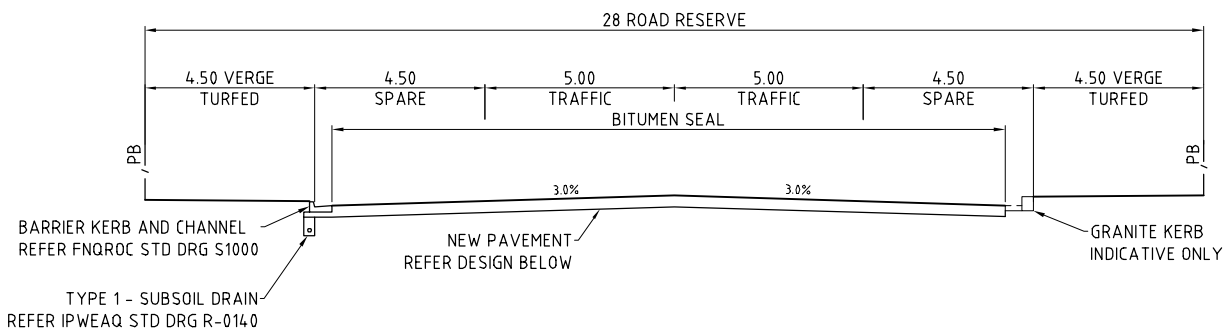
LEGEND

- **EXTENT OF WORKS**
- **EXISTING SEWER MAIN**
- **EXISTING WATER MAIN**
- **EXISTING TELSTRA**
- **EXISTING OVERHEAD ELECTRICITY**
- **EXISTING UNDERGROUND ELECTRICITY**
- **PROPOSED WATER MAIN**
- **PROPOSED SEWER MAIN**
- **PROPOSED UNDERGROUND ELECTRICITY**
- x x x x x **EXISTING SERVICE TO BE ABANDONED**



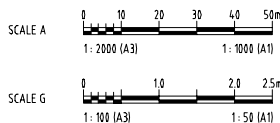
**TYPICAL SECTION P1
SHARED FOOTPATH
INCLUDING VEHICLES**
SCALE G

PAVEMENT DESIGN
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SL 72 MESH CENTRAL
100mm SUBBASE CBR 45

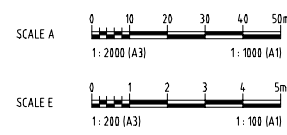
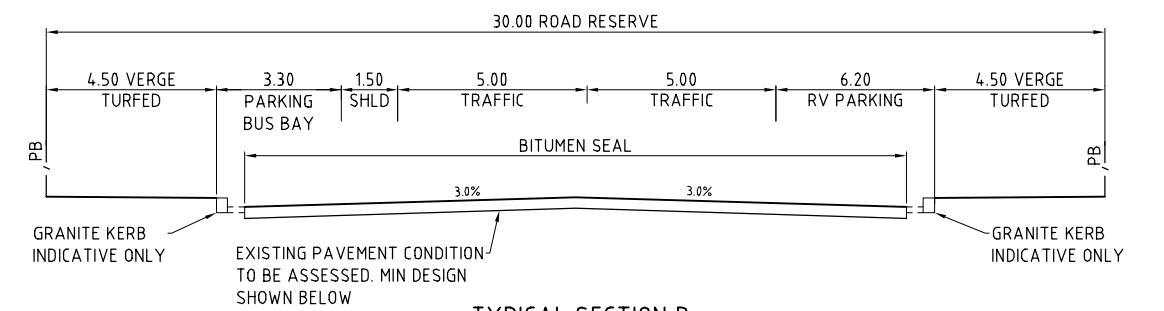
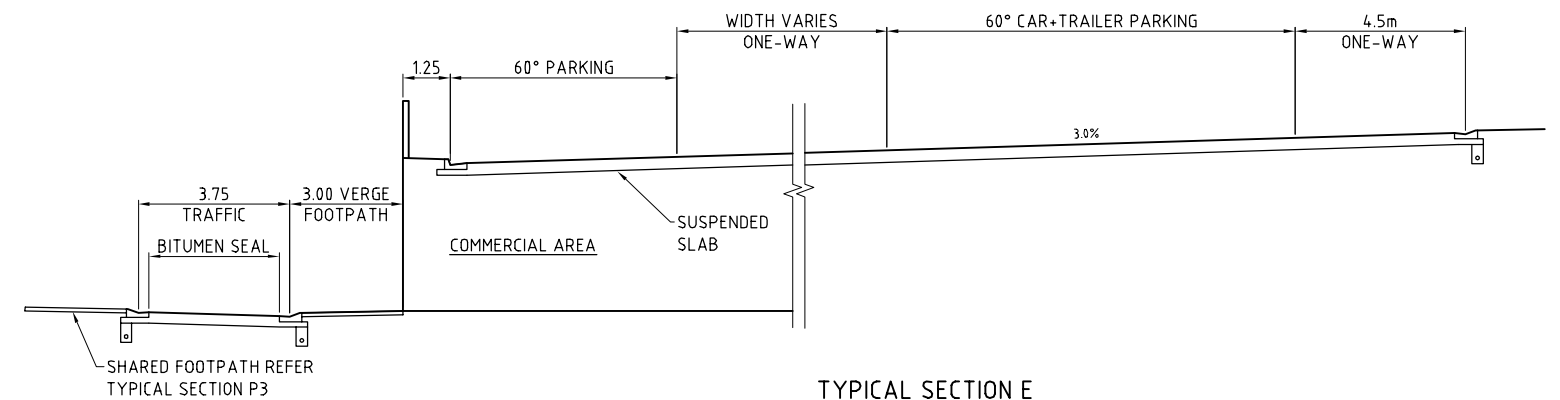
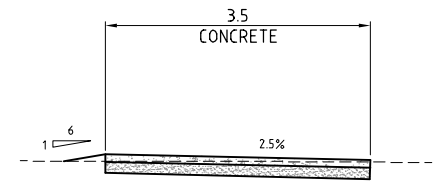
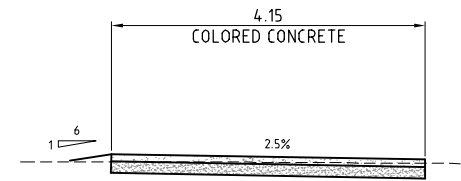
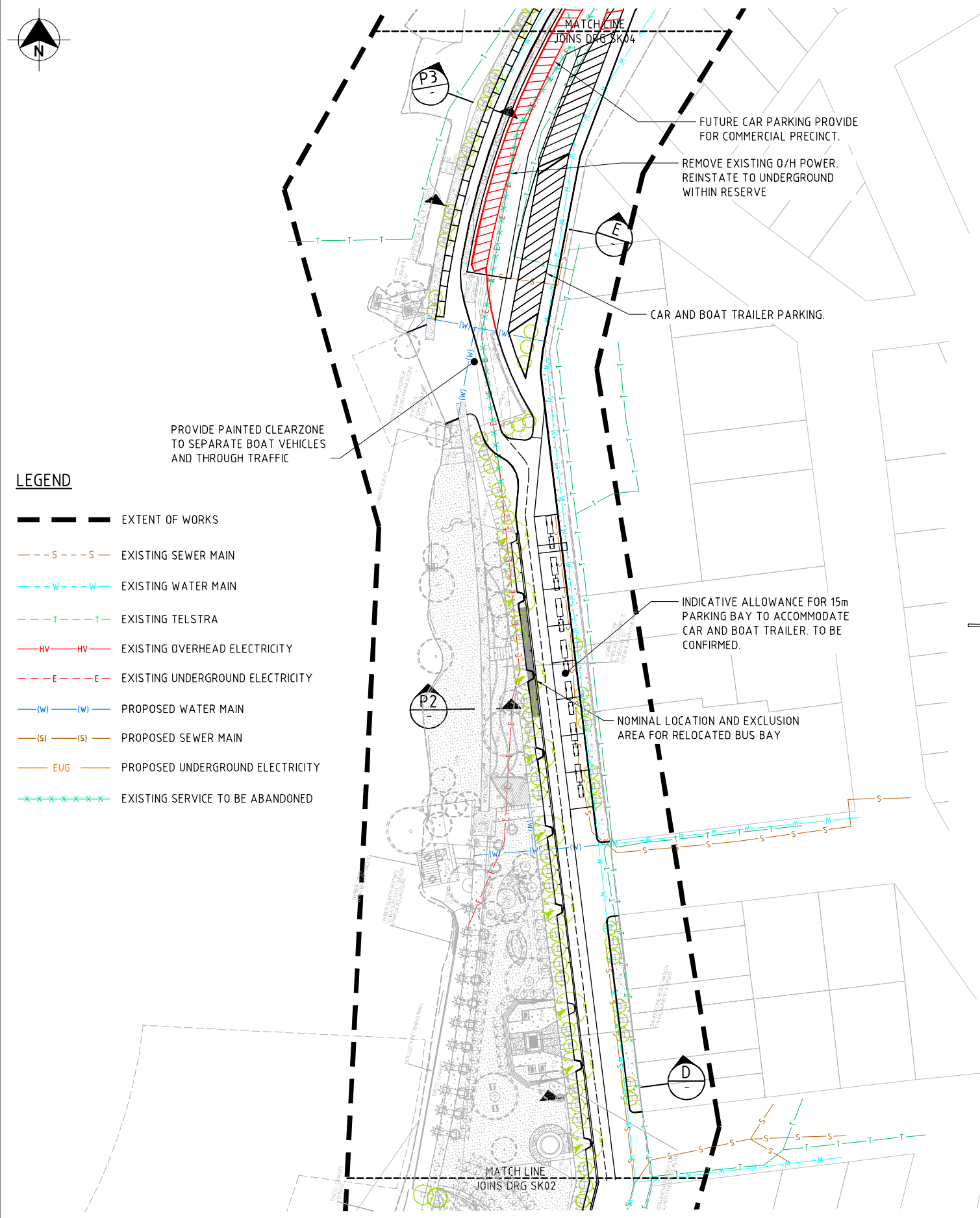


**TYPICAL SECTION C
FURNEAUX STREET**
SCALE E

PAVEMENT DESIGN
2 COAT BITUMEN SPRAY SEAL
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WITH PRIMER SEAL
150mm BASE CBR 60
150mm SUBBASE CBR 45



**CONCEPT DESIGN
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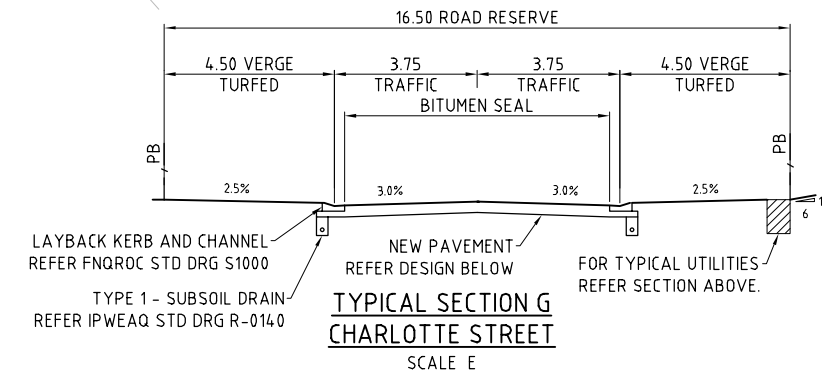
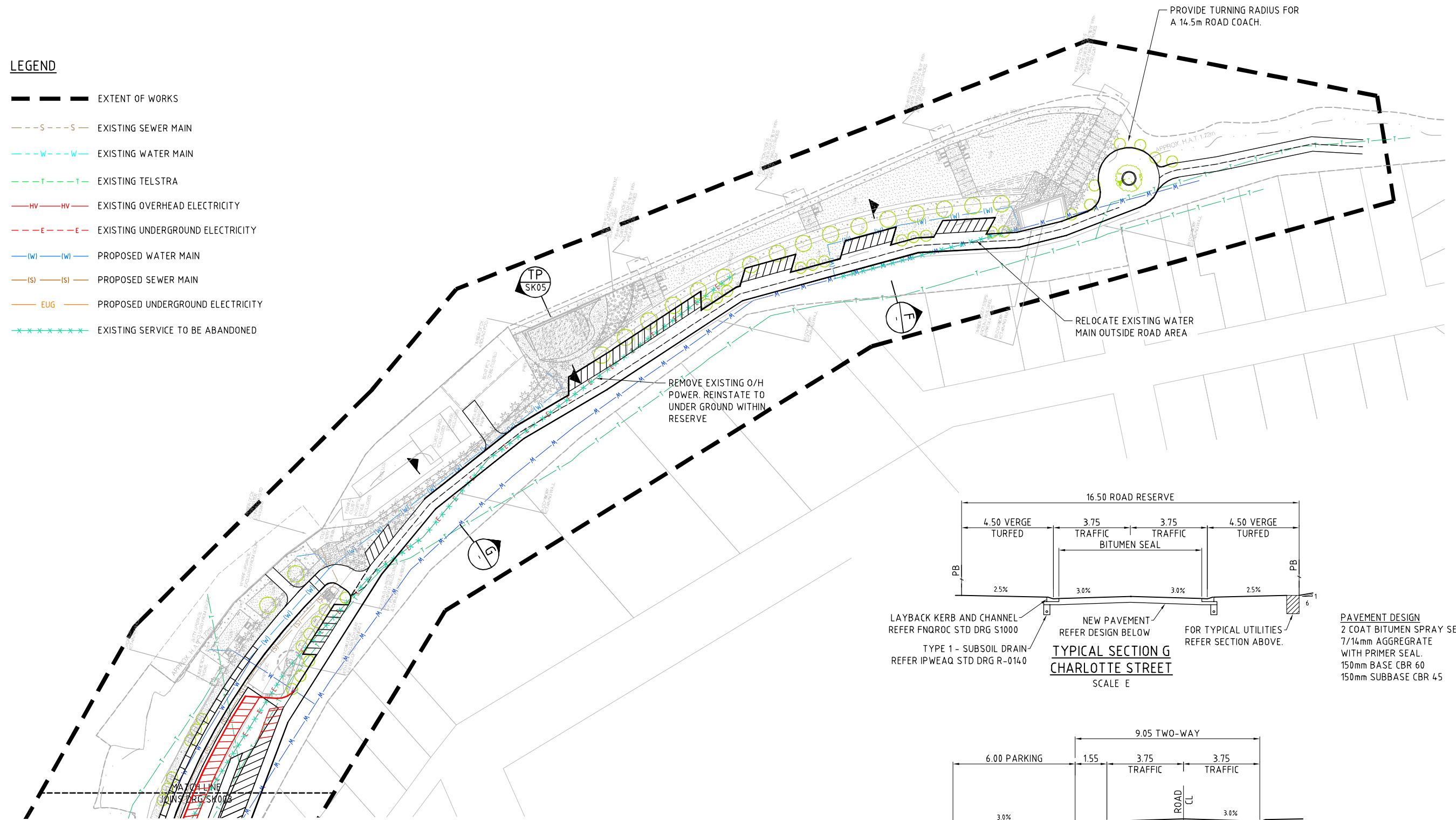


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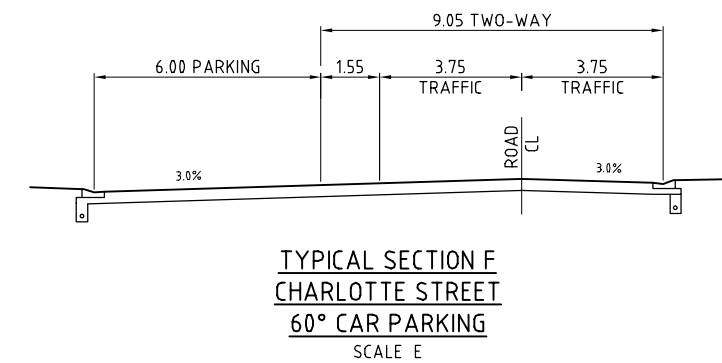


LEGEND

- EXTENT OF WORKS
- - - S - - - EXISTING SEWER MAIN
- - - W - - - EXISTING WATER MAIN
- - - T - - - EXISTING TELSTRA
- HV - HV - EXISTING OVERHEAD ELECTRICITY
- - E - - E - - EXISTING UNDERGROUND ELECTRICITY
- (W) - (W) - PROPOSED WATER MAIN
- (S) - (S) - PROPOSED SEWER MAIN
- EUG - PROPOSED UNDERGROUND ELECTRICITY
- x - x - x - x - x - EXISTING SERVICE TO BE ABANDONED



PAVEMENT DESIGN
2 COAT BITUMEN SPRAY SEAL
7/14mm AGGREGATE
WITH PRIMER SEAL.
150mm BASE CBR 60
150mm SUBBASE CBR 45



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**CONCEPT DESIGN
NOT FOR CONSTRUCTION**

5.8 Staged Implementation

The Cooktown Foreshore Master Plan Staged Implementation Plan has been derived from community consultation results and CSC advice. The community and council have identified which projects they foresee to be the most necessary for the township, as well as what they would like to change or upgrade. The summary of results can be viewed the in Community Consultation section.

The Master Plan has been divided into four stages that follow the perimeters of the foreshore precincts. This approach will enable CSC the opportunity and flexibility to construct the whole precinct in one construction or select particular projects (within the precinct) to obtain funding or approvals. In accordance with the consultation results, the four precinct stages have deliberately not been ordered in implementation priority. This approach enables Council the flexibility to develop various projects along the foreshore as need or funding permits. To reinforce CSC's strategic development vision, internal precinct projects have been strategically ordered to illustrate the implementation process, if a large funding opportunity arises.

The proposed infrastructure upgrades to support this Master Plan have been illustrated per precinct stage. For ease of achieving long term goals and economically efficiency the overall proposed infrastructure upgrades have been costed per precinct stage. This approach minimises the expense of allowances and margins that occur with any project.

In order to establish a holistic approach, deliver upon the design intent to developing or constructing this Master Plan and reflect the community's opinion expressed during consultation, the following precincts are not listed in any particular order.

5.9 Indicative Capital Budgets For Proposed New Infrastructure

5.9.1 Executive Summary

As mentioned, four stages have been identified over the whole site. Within each precinct stage are individual projects. These projects are of manageable size and are more likely to attract ongoing funding and government approval. The precinct stages and their projects have been costed and are as follows:

Overall Master Plan (Estimated Net Cost): \$19,904,245

Overall Master Plan (Incl. Estim. Margins & Adjustments): \$33,080,852

The Estimated Margins and Adjustments figure has been calculated based on allowances made for the delivery of the master plan over a 10 year period.

Passive Natural Recreation Precinct Stage: \$5,210,690

PP1. Foreshore Link	\$215,687
PP2. RV Parking	\$2,023,984
PP3. Exercise Stations	\$36,201
PP4. Reconciliation Rocks	\$415,858
PP5. Rail Shunting Yard	\$481,271
PP6. Mangrove Boardwalk	\$494,453

PP7. Proposed Openspace	\$461,771
PP8. Anzac Park	\$314,173
PP9. Mangrove Boardwalk - Excluded from total	\$433,860
PP10. Mangrove Boardwalk -Excluded from total	\$430,262
PP11. Site Services	\$767,300

Celebration Precinct Stage: \$2,460,217

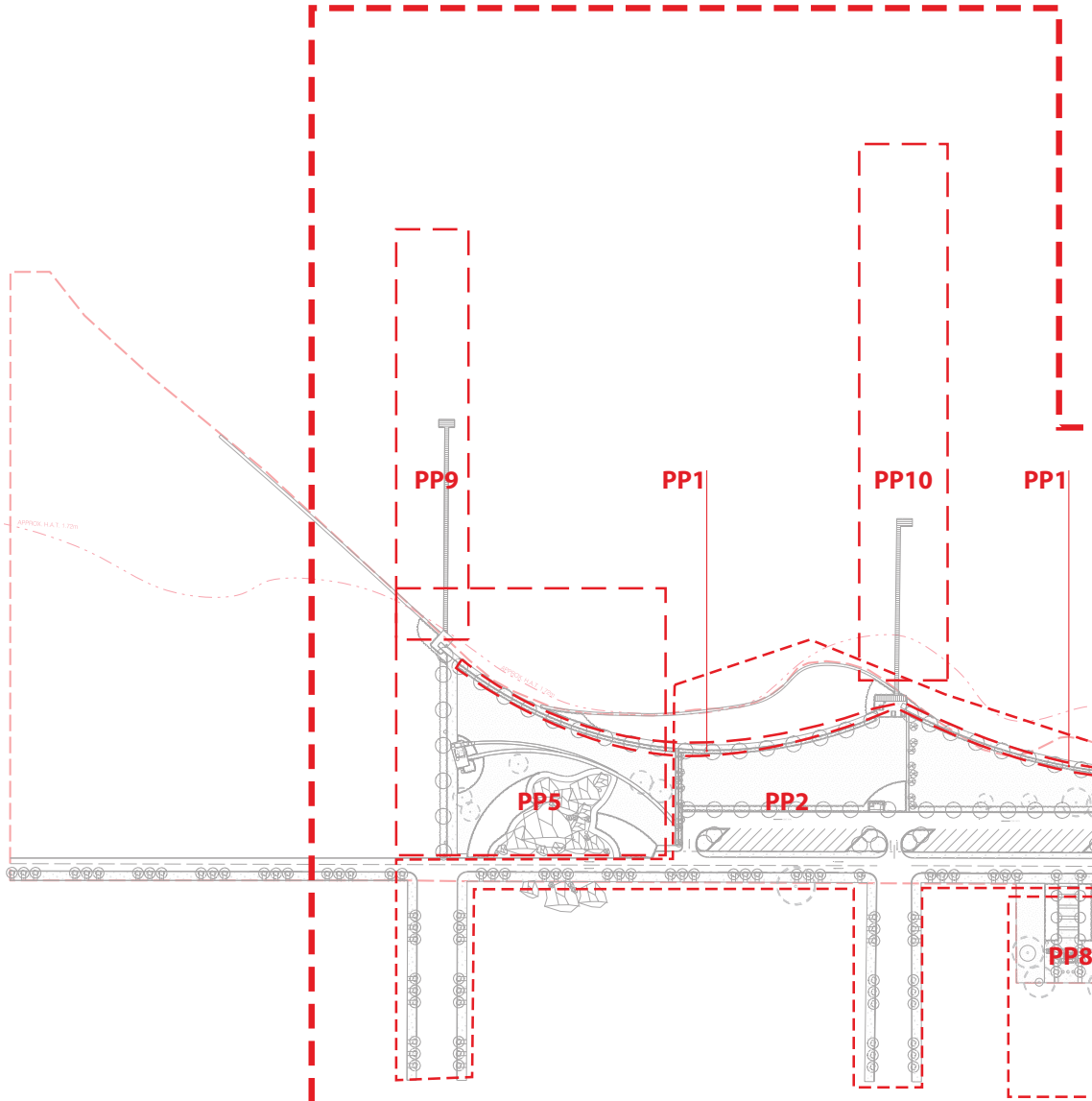
CEP1. Foreshore Link - Shared Path Upgrade	\$909,027
CEP2. Endeavour Park Revitalisation	\$259,187
CEP3. Exercise Stations	\$26,059
CEP4. Captain Cook Memorial Upgrade	\$99,292
CEP5. Lion's Park Revitalisation	\$295,181
CEP6. Openspace Upgrade	\$172,001
CEP7. Site Services	\$699,470

Commercial Precinct Stage: \$6,340,264

CP1. Foreshore Links	\$132,901
CP2. Traffic Management and Parking	\$4,357,256
CP3. Commercial Opportunity - Entry Terminal and Toilets	\$534,592
CP4. Commercial Upgrade - Waste Disposal	\$5,490
CP5. Wharf and Jetty Upgrade	Excluded.
CP6. Openspace Revitalisation - Wharf and Jetty	\$156,204
CP7. Openspace Revitalisation - Queen's Steps	\$43,409
CP8. Boat Ramp Facilities	\$104,282
CP9. Pen Installation	\$563,060
CP10. Site Services	\$443,070

Activity Precinct Stage: \$5,893,074

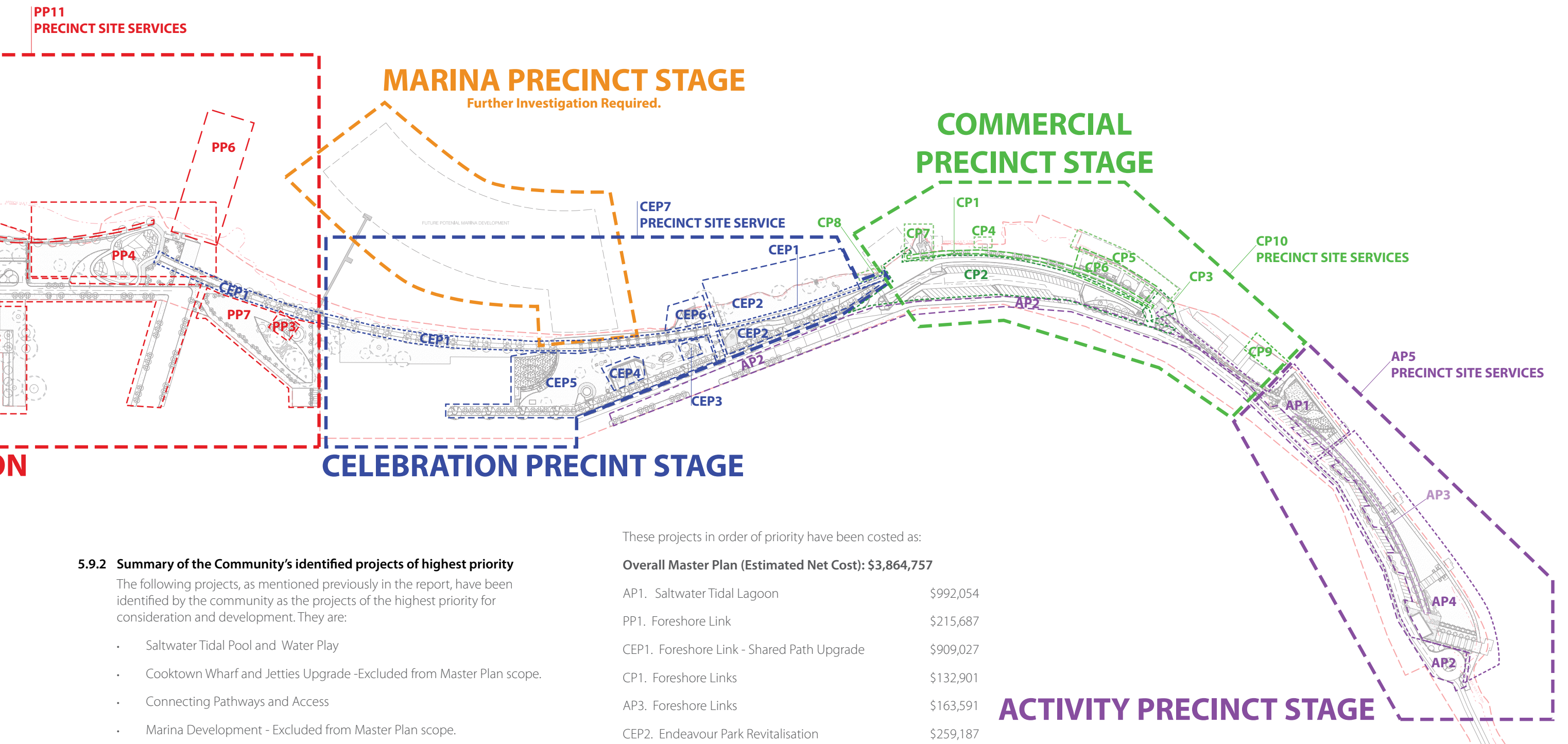
AP1. Saltwater Tidal Lagoon (incl. Waterplay Equipment)	\$992,054
AP2. Traffic Management and Parking	\$2,127,025
AP3. Foreshore Links	\$163,591
AP4. Powder Magazine Openspace	\$957,614
AP5. Site Services	\$1,652,790



PASSIVE NATURAL RECREATION
PRECINCT STAGE

Marina Precinct Stage: (Excluded from Costing)

According to previous studies and Master Plans completed by CSC, this area has been identified as the most appropriate and viable location for a potential future marina. The design and staged implementation of this precinct will require further investigation. For this reason, this precinct has been identified as one whole stage.



5.9.2 Summary of the Community's identified projects of highest priority

The following projects, as mentioned previously in the report, have been identified by the community as the projects of the highest priority for consideration and development. They are:

- Saltwater Tidal Pool and Water Play
- Cooktown Wharf and Jetties Upgrade -Excluded from Master Plan scope.
- Connecting Pathways and Access
- Marina Development - Excluded from Master Plan scope.
- Captain Cook's Landing Area Revitalisation
- Lion's Park Revitalisation
- Reconciliation Rocks Revitalisation
- Rail Shunting Yard Openspace

These projects in order of priority have been costed as:

Overall Master Plan (Estimated Net Cost): \$3,864,757

AP1. Saltwater Tidal Lagoon	\$992,054
PP1. Foreshore Link	\$215,687
CEP1. Foreshore Link - Shared Path Upgrade	\$909,027
CP1. Foreshore Links	\$132,901
AP3. Foreshore Links	\$163,591
CEP2. Endeavour Park Revitalisation	\$259,187
CEP5. Lion's Park Revitalisation	\$295,181
PP4. Reconciliation Rocks	\$415,858
PP5. Rail Shunting Yard	\$481,271

Figure – Staging Implementation Diagram

5.10 Community Identified Projects Overlays

The community consultation process has driven the character, function and future direction of the Cooktown Foreshore. The main functional aspects as considered by the community to have the highest importance to be developed first are listed below.

5.10.1 Increased Seating and Picnic Facilities

- ① CSC aim to deliver 9 additional sheltered barbeque facilities, 21 picnic tables, 33 bench seats, 19 rubbish bins and 8 drinking fountains along the Foreshore and Webber Esplanade. The majority of these proposed seating and picnic facilities are to be installed from Lions Park to the Powder Magazine, making full use of the existing natural shades and water views. Refer to Furniture Plan for more specific qualities and products.

5.10.2 Car and Trailer Parking

- ② The community has identified the need for more regulated parking at main populated areas of the Foreshore. CSC aim to deliver extensive traffic management, mainly within the Commercial and Activity Precinct. To achieve this, the Master Plan proposes designated boat trailer parking in close proximity to the boat ramp, vehicle parking outside commercial establishments, distinct separation of pedestrians and motorists, Charlotte Street parallel parking and lastly, angled parking for the lagoon, Fishing Club and Powder Magazine. Within the Passive Natural Recreational Precinct, self-contained RV short-term parking will encourage more tourism and remove the large vehicles from the town centre movements.

5.10.3 Historical Value - Preserve and Amplify

- ③ This Master Plan respectfully preserves the existing significant monuments, natural landscape and heritage elements that reflect the rich history of Cooktown. This is achieved by:
- Actively retaining the position majority of monuments, memorials within the landscape;
 - Manoeuvring some monuments to celebrate their importance and their association to other surrounding monuments;
 - Physically elevating particular monuments and memorials to amplify their significance within the landscape;



- Interpretive media and stamped feature concrete to illustrate the rich variety of history and culture of Cooktown; and
- Restoration works for particular monuments and memorials to ensure their longevity.

5.10.4 Playground and Exercise Equipment

- ④ CSC aim to deliver two outdoor exercise stations, with three pieces of equipment in each that are fixed to a softfall base. Also proposed, as part of the saltwater tidal lagoon, is waterplay equipment. This waterplay park will feature three zones with all-abilities varied equipment that accommodates toddlers, teenagers and families. Lastly, the existing playground equipment and musical ship in Lions and Endeavour Parks will be retained with upgrades to a softfall base and fencing.

5.10.5 Shade - Structural and Natural

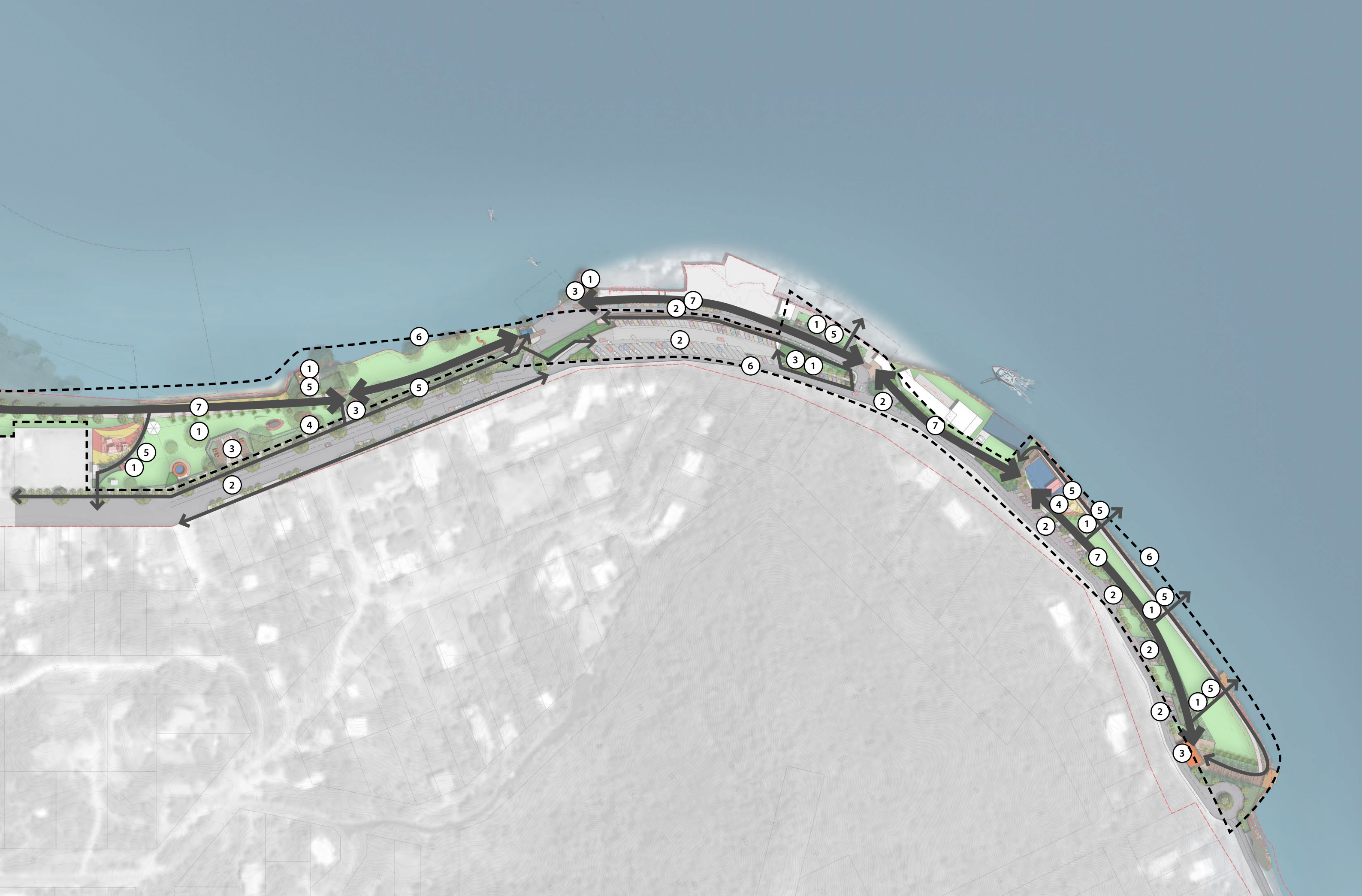
- ⑤ Numerous sheltered barbeque facilities will generate the majority of structural shade along the waterfront. An abundance of shady trees will line the foreshore links and other paths. Street trees along Charlotte, Adelaide, Webber, Hogg, Walker and Furneaux Streets will provide natural shade for pedestrians and parked vehicles. Over the lagoon, a custom shelter will provide protection from the harsh climatic elements. Existing mature vegetation will be retained.

5.10.6 Openspace Upgrade

- ⑥ Part of the Master Plan design intent was to create the foreshore as one long openspace/parkland that enables residents and visitors to be connected to the water's edge and take in the beautiful views. The Master Plan has delivered this by connecting all existing parks along the waterfront, upgrading finishes, infrastructure, planting, and providing more opportunities for recreational activities like swimming, picnicking, deep water fishing, dining, riding/walking and bird watching, to name a few.

5.10.7 Access and Connectivity - Walkways / Cycleways

- ⑦ The Master Plan features 'foreshore links' that equitably connect the main attractions along the length of the foreshore. Portions of these 'foreshore links' are of vehicle grade, to enable restricted vehicle access to property holders and event organisers. Other secondary paths provide direct access between the town centre and the foreshore. Streetscape upgrade enables safe equitable pedestrian movements from town along Charlotte, Adelaide, Webber, Hogg, Walker and Furneaux Streets.



5.11 Funding Streams

The potential sources for future funding for the implementation of this Master Plan have been identified below. Please note that these suggestions should be researched further to ensure that they are applicable and active at the time of application. The following information has been sourced and extracted (word for word) from the associated governing bodies' websites.

Tropical North Queensland: Tourism Opportunity Plan

"The purpose of this Tourism Opportunity Plan (TOP) is to provide direction for the sustainable development of tourism in the Tropical North Queensland region. The TOP aims to:

- Identify new and upgraded tourism product that meets future visitor expectations and demands;
- Identify the need for new investment in infrastructure that supports the ongoing development of tourism;
- Provide relevant research based information on tourism supply and demand; and
- Provide an agreed focus and mechanisms for engagement with the tourism industry, infrastructure and private investors."

"Catalyst Projects

Through the consultation and review process the following 18 catalyst projects have been identified for the Tropical North Queensland region. These projects are tourism investment or infrastructure projects of regional significance which are expected to act as a catalyst to a range of other investment, marketing and product development opportunities.

15. Cooktown Port and Marina"

(Sourced from http://www.tq.com.au/fms//tq_corporate/destinations/tnq/plans_and_strategies/TNQ_TOP-%20FINAL.pdf, 16/01/12)

Regional Development Australia Fund

As described in the guidelines, 'the Regional Development Australia Fund (RDAF) is a national program to support Australia's regions and enhance their wellbeing and economic development. This program is administrated by the Department of Regional Australia, Regional Development and Local Government (the Department) It is designed to ensure that new investments build on strengths and reflect the characteristics, opportunities and challenges of our diverse regions.'

This funding stream program is currently active. CSC have submitted an application for separate works outside of the scope of this Master Plan. However, pending success, and if applicable and in correlation, this funding could be used to indirectly support this purposed Master Plan.

Local Government Grants and Subsidies Program

"From 2011-12 the Queensland Government has committed \$45 million per year, under the Local Government Grants and Subsidies Program (LGGSP), to provide financial support for local governments that demonstrate that they have a limited capacity to self-fund an identified priority project, as evidenced by a financial sustainability evaluation undertaken by the department.

Under the LGGSP, funding is prioritised to projects that align with the Queensland Government's Towards Q2: Tomorrow's Queensland ambitions of:

- Strong - creating a diverse economy powered by bright ideas;
- Green - protecting our lifestyle and environment;
- Smart - delivering world-class education and training;
- Healthy - making Queenslanders Australia's healthiest people; and
- Fair - supporting safe and caring communities.

As part of the Queensland Government's commitment to rebuilding Queensland following the recent significant natural disaster events, priority may be given to projects in affected localities.

The LGGSP program, which incorporates the Climate Ready Infrastructure Initiative, aims to provide funding assistance to councils in Queensland to deliver projects that:

- are identified community needs;
- support the ongoing sustainability of their communities;
- protect natural and built environments;
- enhance the quality of life of people living in rural communities by improving recreational, tourist or cultural facilities, or improving security measures in public places;
- enhance the social, economic and environmental health of communities;
- support delivery of state, local government and community priorities;
- promote collaboration between neighbouring local governments to deliver regional priorities;
- ensure that greenhouse gas reduction and climate change adaptation are key factors in planning and design; and
- promote and provide opportunity to increase Indigenous employment."

"From July 2011, a funding program called the Local Government Grants and Subsidies Program will commence providing smaller councils with 40% of the approved project costs. The program will provide \$45 million each year and funding will be used to support the continuation of capital work on essential projects like sewage treatment plant upgrades, water pipelines and erosion management; all of which contribute to significant long-term community and environmental benefits." (Sourced <http://www.dlgp.qld.gov.au>, 10/02/12)

Heritage Grants and Funding

The major sources of Australian Government funding for activities that benefit Australia's heritage are:

- competitive programs under which eligible parties can apply for their project to be considered as part of a formal funding round; and

- discretionary, ad hoc or non-competitive grants under which funds are approved by the Minister."

(Department of Sustainability, Environment, Water, Population and Communities. 10/02/12)

Community Memorials Restoration Program

"Objective: To help local government authorities and incorporated or formally constituted community organisations responsible for these icons to recognise, repair or restore them. Provider: Department of Public Works, Amount: Max:\$15,000 for cemetery related projects, \$25,000 for memorial related projects. Note: All grants are supplied on a dollar-for-dollar basis. Closing date: Additional grants may be available in the future."

(Sourced www.smartservice.qld.gov.au, 10/02/12)

Our Place, Our Future Funding Initiative

"Objective: To assist members of rural and remote communities to increase their capacity to build more socially, culturally, and economically sustainable communities. Provider: Department of Employment, Economic Development and Innovation. Amount: \$7.42 million has been committed over three years to growing rural and remote community capacity. Closing date: Guidelines and/or dates have not been finalised for the next round."

(Sourced from<https://www.smartservice.qld.gov.au>, 10/02/12)

Community Action Grants

- "2012-13, Business plan. The Australian Government has approved:
- \$60.4 million (GST inclusive) for 202 competitive open call projects to improve, protect and better manage our natural and productive landscapes, including 64 Landcare projects.
 - \$66.4 million (GST exclusive) in base level funding for 22 regional natural resource management (NRM) organisations to protect the Australian landscape and help ensure our farmlands remain productive into the future.

An additional \$38 million (GST exclusive) has been provided to support Reef Rescue projects.

Caring for our Country is the Government's flagship initiative in natural resource management. Over the first five years from 2008-2013 it will provide more than \$2 billion in funding.

Caring for our Country investment helps regional organisations, industry associations, community groups including Indigenous groups and landholders to protect and conserve Australia's natural resources including our farming lands, coasts, rivers and wetlands, plants and wildlife. The Australian Government received 584 applications for the open call component of the Caring for our Country business plan 2011-12. The initiative coordinates projects across the country to achieve national targets which are backed by an annual business plan to focus investment and deliver maximum results." (Sourced <http://www.dlgp.qld.gov.au>, 10/02/12)



Figure – Queensland Coastal Plan

5.12 Audit Statutory Provisions and Approval Process

A range of approvals will be required in order to allow/facilitate implementation of the foreshore Master Plan. The exact applications required to be made at any given time will depend on a number of factors, including the timing and extent of the works proposed to be undertaken and their likely impacts.

The following outline seeks to identify those application/approval matters which are likely to be relevant at the time of preparing this Master Plan document.

Those actually responsible for Master Plan implementation in the future should undertake their own detailed assessment of approval requirements, based on the circumstances existing at that time.

Specific consideration will need to be given to approval timelines and supporting information requirements.

The proposed foreshore redevelopment works will partly occur on land below Highest Astronomical Tide (HAT). HAT is mapped by the Bureau of Meteorology as being located at 1.72 metres on AHD. The redevelopment will, therefore, trigger a development application for Operational Works – Prescribed Tidal Works. The application will be made to the Council to be assessed against the Prescribed Tidal Works Code of the Coastal Management Regulation 2003. The application will need to have regard for the underlying zoning in the Planning scheme.

The application will also be referred to the Department of Environment and Resource Management for assessment against the Queensland Coastal Plan. The site is partially identified within a maritime development area, a specified area under the Queensland Coastal Plan, State Planning Policy 3/11: Coastal Protection (SPP 3/11). The specified area as identified in SPP 3/11 is the Cooktown marine precinct (MDA_002_003). At present, coastal hazard mapping does exist for the area, however, for development to occur in the area the identified steps within Annex 4 of SPP 3/11 must be adhered to, which generally requires a Management Plan to be established that incorporates consideration of the following legislation:

- Nature Conservation Act 1992;
- Fisheries Act 1994;
- State Planning Policy 2/02: Planning and Managing Acid Sulphate soils;
- Marine Parks Act 2004;
- Queensland Heritage Act 1992;

- Environmental Protection (Water) Policy 2009, schedule 1, column 1 and column 2;
- Wild Rivers Act 2005; and
- Vegetation Management Act 1999.

There is also potential for referral to the Commonwealth Department of Sustainability, Environment, Water, Population and Communities for assessment against the Environment Protection and Biodiversity Conservation Act 1999.

The CSC must also discharge its duty of care under the Aboriginal Cultural Heritage Act 2003 and Torres Strait Islander Cultural Heritage Act 2003 in relation to the protection of potential and/or existing cultural heritage features.

It appears that the majority of the redevelopment is planned to occur on esplanade or unallocated State land. It will, therefore, be necessary to attain resource entitlement from the State Land Asset Management Branch of the Department of Environment and Resource Management, prior to lodging the development application.

Any development application involving a material change of use and/or reconfiguring a lot will need to be assessed against the requirements contained within the Planning Scheme for Cook Shire.







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